



600

West Commonwealth Specific Plan

September 2016

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Table of Contents

Section 1 Introduction

Executive Summary	1-1
1.1 Location	1-1
1.2 Planning Context	1-1
1.2.1 Specific Plan Area.....	1-1
1.2.2 Existing Uses	1-4
1.2.3 Surrounding Uses	1-4
1.2.4 Existing General Plan Land Uses/Existing Zoning.....	1-5
1.2.5 Soils and Geology	1-6
1.2.6 Infrastructure and Utilities	1-6
1.3 Purpose and Objectives	1-6
1.3.1 Vision	1-6
1.3.2 Project Objectives	1-7
1.4 Legal Context	1-7
1.4.1 Authority and Requirements	1-7
1.4.2 Relationship to the General Plan and Zoning Code.....	1-8
1.4.3 Consistency with the General Plan (The Fullerton Plan)	1-9
1.5 Plan Organization.....	1-13

Section 2 Development Plan and Land Use Regulations

2.1 Land Use Plan	2-1
2.1.1 Multifamily Residential	2-2
2.1.2 Commercial Uses.....	2-2
2.1.3 Recreation and Amenities.....	2-2
2.2 Circulation Plan.....	2-5
2.2.1 Regional Circulation.....	2-5
2.2.2 Arterials.....	2-6
2.2.3 Local Streets	2-6
2.2.4 Site Access	2-6
2.2.5 Transit.....	2-7
2.2.6 Parking.....	2-7

2.3	General Provisions	2-7
2.3.1	Applicability	2-8
2.3.2	Severability	2-8
2.3.3	Determination of Unlisted Uses/Interpretation	2-8
2.3.4	Definitions	2-8
2.3.5	Use of Design Guidelines	2-9
2.3.6	Subsequent Building Modification.....	2-9
2.3.7	Projections into Required Yards	2-9
2.3.8	Height.....	2-9
2.4	Residential Uses.....	2-9
2.4.1	Permitted Uses	2-10
2.4.2	Uses Subject to a Conditional Use Permit	2-10
2.4.3	Development Standards	2-10
2.5	Commercial Uses	2-10
2.5.1	Permitted Uses	2-11
2.5.2	Development Standards	2-11
2.6	Parking	2-13
2.7	Signage	2-13

Section 3 Urban Design

3.1	Architecture.....	3-1
3.1.1	Architectural Design Inspiration	3-1
3.1.2	Mass and Form	3-11
3.1.3	Façade Articulation	3-11
3.1.4	Materials and Colors	3-11
3.1.5	Safety Through Design	3-12
3.2	Landscape Architecture.....	3-13
3.2.1	Landscape Master Plan	3-13
3.2.2	On-Site Gathering Spaces	3-13
3.2.3	Utility Placement and Screening	3-14
3.3	Walls and Fences	3-14

Section 4 Infrastructure and Services

4.1	Infrastructure Plan	4-1
4.1.1	Water	4-1
4.1.2	Wastewater	4-1
4.1.3	Drainage and Water Quality.....	4-3
4.1.4	Utilities	4-5
4.2	Public Services	4-6
4.2.1	Fire and Police	4-6
4.2.2	Solid Waste	4-6
4.2.3	Schools	4-6

Section 5 Implementation

5.1	Administration	5-1
5.1.1	Responsibility.....	5-1
5.1.2	Applicability	5-1
5.1.3	Enforcement.....	5-1
5.1.4	Severability	5-1
5.1.5	Interpretation.....	5-2
5.1.6	Initial Entitlements.....	5-2
5.1.7	Substantial Conformance.....	5-2
5.1.8	Amendments to the Specific Plan	5-4
5.1.9	Appeals	5-4
5.2	Implementation	5-4
5.2.1	Specific Plan Adoption	5-4
5.2.2	Subsequent Approvals and Plans.....	5-5
5.3	Phasing	5-5
5.4	Maintenance and Ownership	5-5
5.5	Financing	5-6
5.6	Relationship to CEQA.....	5-6

1 **INTRODUCTION**

This chapter explains the purpose of the Specific Plan; background; planning process and entitlements; guiding principles; organization of the Specific Plan; authority to prepare; relationship to existing plans and policies; local and regional context and setting.

Executive Summary

The land use concept for the 600 West Commonwealth Specific Plan includes the following primary elements:

- Demolition of existing on-site buildings and parking areas;
- Development of 295 multi-family units with a mix of studio, one-bedroom, two-bedroom, and three-bedroom apartment units. The residential uses are located on two parcels;
- Up to 4,822 square feet of local-serving retail commercial is planned on Parcel A facing West Commonwealth Avenue;
- Structured parking with five to six levels would be provided on Parcels A and B with a total of 589 parking spaces;
- Recreational amenities to support the residential uses;
- Construction of infrastructure improvements to support the proposed land uses.

The Specific Plan has been prepared pursuant to the provisions of California Government Code Section 65450 through 65457, which grants local government agencies the authority to prepare specific plans for any area covered by a General Plan.

When site-specific development proposals for the Specific Plan area are presented to the City, planning staff will use this Specific Plan as a policy and regulatory guide for subsequent project review. Projects will be evaluated for consistency with Specific Plan policies and for conformance with its development standards and design guidelines.

1.1 LOCATION

The project site is located in the City of Fullerton, Orange County, California. The approximately 4.79-acre site is generally bound by West Commonwealth Avenue to the north; the Amtrak and Metrolink rail lines to the south; Chestnut Place to the east; and off-site land uses and South Woods Avenue to the west. The site is bisected by Williamson Avenue and consists of two parcels.

Regional access to the site is provided by State Route (SR) 91, which is located approximately 1 mile south of the site, and SR-57, which is located approximately 3.2 miles east of the site. Local access is provided by West Commonwealth Avenue, North Harbor Boulevard, and Euclid Street. The project site is shown in a regional and local context on Figure 1-1. An aerial view of the site and surrounding area is provided on Figure 1-2.

1.2 PLANNING CONTEXT

1.2.1 Specific Plan Area

The site is bisected by Williamson Avenue and consists of two parcels. Parcel A is located north of Williamson Avenue (Assessor Parcel Numbers [APNs] 032-081-10, 032-081-26, 032-081-29, and 032-081-30), and Parcel B is located south of Williamson Avenue (APN 032-084-04).

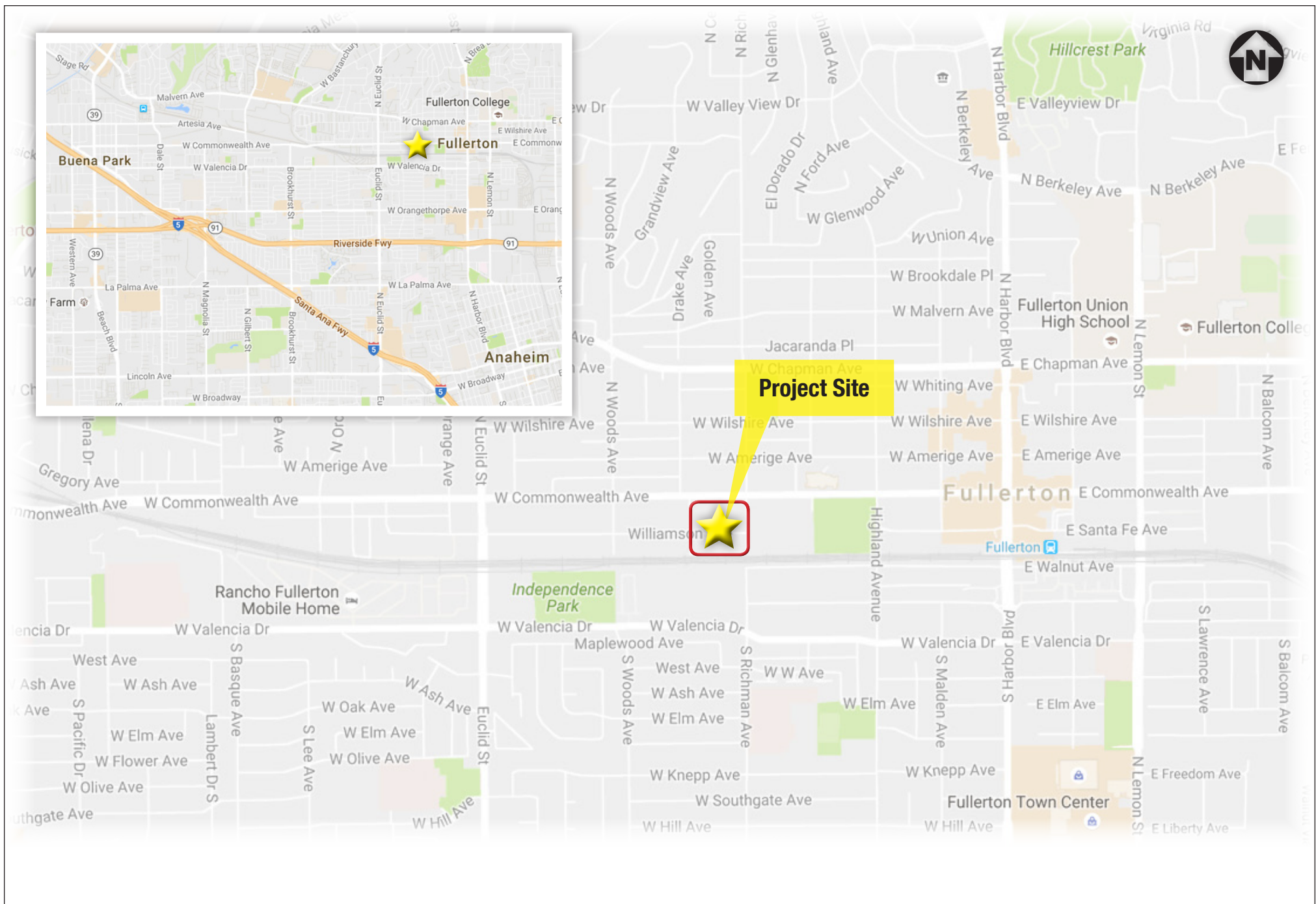
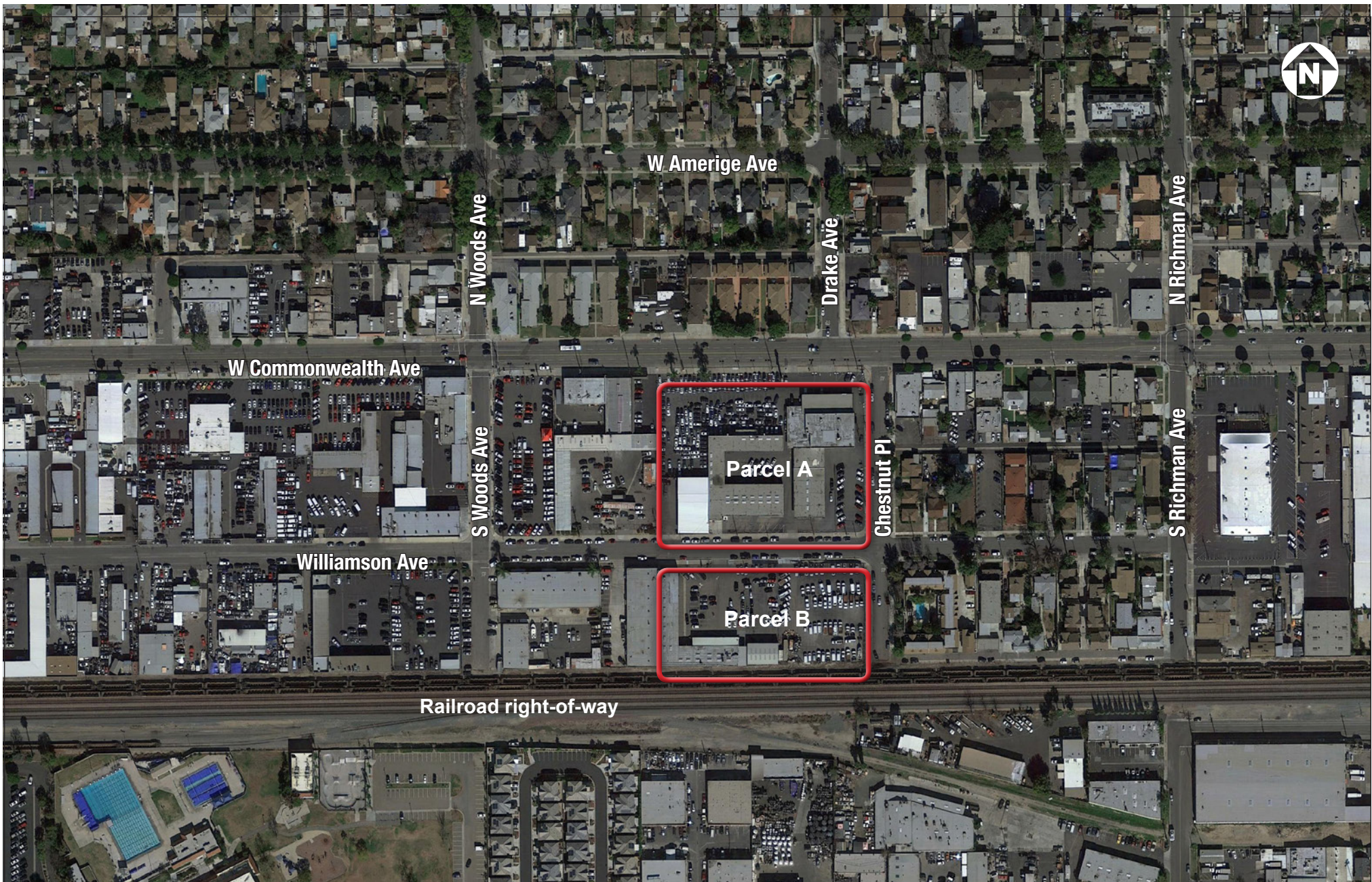


Figure 1-1: Regional and Local Vicinity Map



 Project Site

Figure 1-2: Aerial View

1.2.2 Existing Uses

The Specific Plan area is currently developed and contains six buildings and surface parking areas.

- Parcel A (north of Williamson Avenue). Parcel A includes four buildings associated with a collision center and auto repair company, totaling 33,238 square feet (sf).
- Parcel B (south of Williamson Avenue). Parcel B has been used by a towing company and the auto repair company located on Parcel A. Two connected buildings are used as garages for the auto repair company and total 11,600 sf. Parcel B is fenced with chain link.

Vehicular access to the Specific Plan area is provided at driveways along West Commonwealth Avenue, Williamson Avenue, and Chestnut Place.

The area surrounding the project site contains a mix of smaller retail, service and office uses, and single-family and multi-family residences.

1.2.3 Surrounding Uses

Land uses in the immediate vicinity of Parcel A include the following:

North: Single-family detached residences and automotive repair uses are located to the north of West Commonwealth Avenue. On-street parking is permitted along West Commonwealth Avenue.

South: Parcel B is to the south of Williamson Avenue and is used for automotive repair and by a towing company. On-street parking is permitted along Williamson Avenue.

East: On the east side of Chestnut Place and fronting onto West Commonwealth Avenue, uses include small, one-story professional and commercial uses. Between the alley (south of the businesses) and Williamson Avenue, there are three single-family residences that front onto Chestnut Place.

West: West of the project site to Woods Avenue, the remainder of the block includes a small, one-story professional services building fronting onto West Commonwealth with off-street parking. An auto dealership is located between the building and Woods Avenue and extends from West Commonwealth Avenue to the north to Williamson Avenue to the south.

Land uses in the immediate vicinity of Parcel B include the following:

North: Parcel A includes an auto dealership and auto repair company located between West Commonwealth Avenue to the north and Williamson Avenue to the south.

South: Metrolink and Amtrak railroad tracks.

East: East of Chestnut Place is a multi-family apartment complex that fronts onto Williamson Avenue.

West: West of the project site to Woods Avenue. The remainder of the block includes a professional and automotive services development fronting onto Williamson Avenue.

1.2.4 Existing General Plan Land Uses/Existing Zoning

Existing General Plan Land Uses

The Fullerton Plan, the City's General Plan, was adopted on May 1, 2012. It identifies 12 geographic Focus Areas in the City within which to concentrate future community-based planning efforts. The Specific Plan area is located within The Fullerton Plan's Focus Area B: Commonwealth Corridor, which extends along Commonwealth Avenue east to State College Avenue at Cal State Fullerton, and west to the Fullerton Municipal Airport near Magnolia Avenue.

The Commonwealth Corridor includes distinguishable areas including the five colleges which anchor the east end of the corridor and the airport to the west, with downtown Fullerton and the Civic Center area centered along the corridor. From east to west along Commonwealth Avenue, the area transitions from a revitalizing downtown area, to the project site, located in an area characterized as a mix of smaller retail, service and office uses, and residential development.

The Fullerton Plan states that the *"Commonwealth Corridor Focus Area is envisioned as a complementary mix of retail and commercial uses connecting the City's major activity centers. The Commonwealth Corridor will continue to offer neighborhood-serving retail businesses, while also providing new housing opportunities. Growth will be focused into nodes, each with a distinct sense of place. The corridor as a whole will be enhanced with unifying streetscape improvements and better pedestrian and bicycle access."* The Fullerton Plan's objectives include the encouragement of new housing opportunities west of Harbor Boulevard, and the promotion of a unified character through unique streetscape design.

A General Plan Amendment is required prior to approval of this Specific Plan to revise the designation to Urban Center Mixed Use. Upon approval of that amendment, the Specific Plan would be consistent with the General Plan.

Existing Zoning

Parcel A is zoned General Commercial (G-C), a category added to the zoning code in August 2016. The Fullerton Municipal Code defines the G-C zone as a zone "to provide for a wide range of commercial goods and services" The G-C zone does not permit mixed-used development. Parcel B is zoned Manufacturing, General (M-G) which is intended to allow for more intensive industrial uses. Residential uses and mixed-use development with residential uses are not permitted in the M-G zone. As a part of the proposed project, the zoning designation for Parcels A and B would be changed to Specific Plan District (SPD).

Fullerton Municipal Code Chapter 15.21 states "The purpose of the Specific Plan District (SPD) zone is to provide for the orderly, systematic, detailed and enhanced implementation of particular areas of the General Plan where a variety or combination of land uses are being proposed over a substantial amount of land. The Specific Plan District (SPD) zone provides for the establishment of physical development standards and regulations for land uses that may be unique to the particular area where a Specific Plan is being adopted."

1.2.5 Infrastructure and Utilities

Wastewater service is provided by the Orange County Sanitation District (OCSD). The OCSD currently provides wastewater service to the project area including the existing on-site uses. Existing OCSD trunk sewer lines are located in West Commonwealth and Williamson Avenues.

The City of Fullerton provides water service. Water service to the site is presently available from an existing 12-inch water line in West Commonwealth Avenue and an existing 8-inch water line in Williamson Avenue.

Southern California Edison (SCE) provides electricity to the project site through existing power lines on the site along the western property line.

1.2.6 Geology and Soils

Based on information provided by a geotechnical investigation by Geocon West, Inc. in 2015, the subject site is situated in the northern portion of the Orange County Coastal Plain in the physiographic feature known as the Lower Santa Ana River Basin. This basin is part of an elongated east-west trending structural depression bounded by the Norwalk Fault to the north, the Santa Ana Mountains to the east, the Orange/Los Angeles county line to the west, and the Pacific Ocean to the south. The sediments which make up the Lower Santa Ana River Basin consist primarily of alluvial fan deposits derived from erosion and deposition of sediments by the Santa Ana River. Topographically the Lower Santa Ana River Basin slopes generally southwest from the Puente Hills and Santa Ana Mountains. The site is underlain by artificial fill and Holocene age alluvial fan deposits consisting of varying amounts of unconsolidated silt, sand, and gravel.

The site is not within a currently established Alquist-Priolo Earthquake Fault Zone for surface fault rupture hazards. No active or potentially active faults with the potential for surface fault rupture are known to pass directly beneath the site.

The project site is relatively flat with an approximately 0.5 percent grade. The property is at approximately 143 feet above mean sea level (msl).

1.3 PURPOSE AND OBJECTIVES

1.3.1 Vision

The City of Fullerton was conceived and founded due to the railroad. In 1887 California Central Railroad, a subsidiary of Santa Fe, was looking for land and sent George Fullerton, President of the Land and Improvement Company, to purchase land for the railroad. George and Edward Amerige learned of a like town site north of Anaheim and after conversations with Fullerton, bought the property. And on July 5th, 1887, the city was born. The appreciative community voted to name the city after its benefactor, George Fullerton.

The train is a part of the cohesive fabric that makes up Fullerton, even today. From Fullerton, one can connect to shops, restaurants, and entertainment, and Los Angeles or San Diego are within reach due to mass transit. With the site so close to the train station -- literally following the tracks, a train motif was followed for the project.

The theme of motion ties itself well into the history and urban fabric of Fullerton, but it also plays well with how we live, work and play today. Our society is faster paced today than in the 1800's, and seems to be getting faster every day. Motion, in a variety of aspects, is a part of our lives, in how we get to our favorite restaurant, to posting a review on our favorite internet site. We are a more mobile people, from using the car, train, bicycle, walking, hover boards, etc. The project's design vision is grounded in Fullerton's past, but also in its future, and serves as a statement of motion in our present lives.

1.3.2 Project Objectives

The 600 West Commonwealth Specific Plan has been prepared within the framework of a detailed design program. Issues such as high design standards and engineering feasibility have been evaluated in developing the design program. This process has resulted in a set of specific project planning objectives, outlined below:

1. Build a neighborhood that helps implement the City's vision as expressed in the goals of the Commonwealth Corridor Focus Area of The Fullerton Plan.
2. Replace aging businesses on two underutilized infill sites with a mixed use project.
3. Replace outdated, energy inefficient buildings with new construction that meets current energy standards.
4. Make Williamson Avenue a more pedestrian friendly environment.
5. Provide for well-managed parking, located unobtrusively.
6. Improve overall water quality by treating runoff on a site that currently provides no treatment.
7. Provide housing compatible with the City's vision for areas near Downtown.
8. Develop housing within walking and biking distance (.5 miles) of Downtown.
9. Provide for publicly accessible plaza and courtyard spaces to improve the urban fabric in the project vicinity.
10. Consolidate two parcels into a single development area to allow for a comprehensive and unified development plan.

1.4 LEGAL CONTEXT

1.4.1 Authority and Requirements

A "Specific Plan" is a planning and regulatory tool made available to local governments by the State of California. Specific plans implement a city's General Plan through the development of policies, programs, and regulations that provide an intermediate level of detail between General Plans and individual development projects. State law stipulates that specific plans can only be adopted or amended if they are consistent with a city's adopted General Plan.

The 600 West Commonwealth Specific Plan implements the goals and policies of the General Plan, serves as an extension of the General Plan, and can be used as both a policy and a regulatory document. The purpose of this Specific Plan is to implement the vision by providing goals, policies, programs, development standards, and design guidelines to direct future development within the Specific Plan Area.

The authority to prepare and adopt a Specific Plan and the requirements for its contents are set forth in California Government Code Sections 65450 through 65457. Section 65451 states:

A Specific Plan shall include a text and a diagram or diagrams which specify all of the following in detail:

- The distribution, location and intent of the uses, including open space, within the area covered by the plan.
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described by the plan.
- Standards and criteria by which the development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- A program of implementation measures including programs, public works projects, and financing measures.
- The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

1.4.2 Relationship to the General Plan and Zoning Code

General Plan

A detailed consistency analysis of the goals and policies with the general plan can be found in the Appendix of this document.

Together with the Specific Plan, the City of Fullerton has the discretion to approve the necessary amendments to the existing General Plan to provide for the land uses, goals and policies described in this Specific Plan. In situations where policies or standards relating to a particular subject have not been provided in this Specific Plan, the policies and standards in the General Plan will continue to apply.

Zoning Ordinance

The Fullerton Zoning Code (the Zoning Ordinance) provides a citywide framework of regulations that addresses topics such as permitted uses, development standards, parking and landscaping regulations, permit procedures, and sign regulations.

This Specific Plan includes the development standards that will apply to the Specific Plan area. In situations where policies or standards relating to a particular subject are silent in this Specific Plan, the existing regulations of the Zoning Ordinance will continue to apply. In the event that any provision in this Specific Plan conflicts with Zoning Ordinance, the provisions set forth within the Specific Plan shall prevail.

The approval of the Specific Plan would include an amendment to both the General Plan and Zoning designations for the site to allow the development of 295 multifamily units and approximately 4,822 square feet of commercial uses. To accomplish this, a Zone Change and General Plan Amendment will be required:

- **General Plan Amendment.** The existing Fullerton General Plan designates Parcel A as “Commercial” and Parcel B as “Industrial.” A proposed General Plan Amendment would change the General Plan land use designation for both parcels to “Urban Center Mixed-Use.”
- **Zoning Amendment.** Parcel A is presently zoned General Commercial (G-C), and Parcel B is zoned Manufacturing, General (M-G). The proposed project would amend the zoning designation on the project site (Parcel A and Parcel B) to “Specific Plan District (SPD).”

1.4.3 Consistency with the General Plan (The Fullerton Plan)

The Specific Plan must be consistent with The Fullerton Plan. A detailed discussion of the Specific Plan’s consistency with The Fullerton Plan, as amended by the project’s General Plan Amendment, is provided below. Subsequently, all future development plans on the property must be consistent with the Specific Plan. Projects that are found to be consistent with Specific Plan will be deemed consistent with The Fullerton Plan.

The Fullerton Plan is divided into four sections: The Fullerton Built Environment, The Fullerton Economy, The Fullerton Community, and the Fullerton Natural Environment. An analysis of The Fullerton Plan reveals that 600 West Commonwealth is consistent with the Plan and helps the City achieve many of its stated goals and policies.

Applicable to the Specific Plan, The Fullerton Plan’s objectives for the Commonwealth Corridor include the encouragement of new housing opportunities west of Harbor Boulevard, and the promotion of a unified character through unique streetscape design.

The Fullerton Built Environment

Goal 1: Resilient and vital neighborhoods and districts.

Policy 1.4 – Connection and Integration of Uses: Support projects, programs and policies to improve connections between housing, shops, work places, schools, parks and civic facilities, and integrate uses where possible and appropriate.

Policy 1.9 – Housing Choice: Support projects, programs, policies and regulations to create housing types consistent with market demand for housing choices.

Policy 1.10 – Focus Area Planning: Support projects, programs, policies and regulations to evaluate ways to contribute to the resiliency and vitality of neighborhoods and districts as part of community-based planning of Focus Areas.

The proposed project requires the demolition of the existing automobile-oriented businesses on the site to develop a mixed-use development on the site. The project would include a mix of multi-family housing types including studios, 1-bedroom, 2-bedroom, and 3-bedroom residential units. Surrounding residential uses include both single-family and multi-family housing. Implementation of the proposed project would

help fulfill The Fullerton Plan's objective for new housing located west of Harbor Boulevard in the Commonwealth Corridor.

The Fullerton Built Environment

Goal 2: A positive identity and distinctive image.

Policy 2.6 – Focus Area Planning: Support projects, programs, policies and regulations to create a positive identity and distinctive image as part of community-based planning of Focus Areas.

Policy 2.7 – Relationship to Street: Support projects, programs, policies and regulations to site and design buildings to create a positive, accessible image along the street and reinforce a vibrant and comfortable public realm.

The project would change the site from automobile-oriented businesses and uses to a mixed-use development. The proposed mixed-use development would range from two to four levels at approximately 51 feet in height. The parking structures would be located in the center of the parcels and would not be visible to nearby residential uses. The modern articulated facades of the proposed buildings would include a mix of metal, fiber cement, and plaster elements. The ground floor of Building 1 would have transparent retail storefront windows along the northern façade, which would front Commonwealth Avenue. The southern façade of Building 2, which fronts the railroad right-of-way, would include a painted art mural.

The Fullerton Built Environment

Goal 3: A supply of safe housing ranging in cost and type to meet the needs of all segments of the community.

Policy 3.4 – Facilitate Infill Development: The built-out nature of the City requires the evaluation of land currently developed with existing uses for potential residential development. The City will continue to facilitate infill development within feasible development sites for homeownership and rental units through proactive and coordinated efforts with the Successor Agency, Planning Division, private development and non-profit entities, and other housing-related groups to encourage the construction of housing affordable to extremely-low, very-low, low-, and moderate-income households through a menu of regulatory incentives (e.g., streamlined review, reduced development standards, land assemblage, lot consolidation, fee assistance, and other methods) that will effectively encourage infill development.

Policy 3.5 – Encourage Mixed Use Development: Due to the limited vacant land resources and the desire of the City to provide connections with jobs, housing, and transportation, the City shall continue to encourage mixed use development to further enhance the viability and success of residential development. Key focus areas shall include the City's primary activity centers, including the downtown area. The Fullerton Plan, adopted in 2012, identifies 12 focus areas in which development character is either in transition or desired. Within 11 of these areas, residential development is established as an appropriate use. The Fullerton Plan further includes density parameters for each focus area, establishing maximums ranging from 30 to 80 units per acre, and creates to additional land use designations for mixed use developments. The City will continue to encourage mixed use development in the C-3 zone and the two additional Mixed-Use Community Development Types. It is anticipated that future mixed use development may be implemented by specific plan. The City will further encourage mixed use

development through a variety of activities such as organizing special marketing events geared towards the development community, posting the sites inventory on the City's webpage, identifying and targeting specific financial resources and reducing appropriate development standards.

The project proposes a mixed-use development to replace the existing automobile-oriented businesses located on the project site. The project would provide a range of housing options, including studios, 1-bedroom, 2-bedroom, and 3-bedroom residential units.

The Fullerton Built Environment

Goal 5: A balanced system promoting transportation alternatives that enable mobility and an enhanced quality of life.

Policy 5.14 – Fair Share Improvements: Support policies and regulations which require new development to pay a fair share of needed transportation improvements based on a project's impact to the multi-modal transportation network.

The project site is located along an OCTA bus line (Route 26) which connects the project to the Fullerton Transportation Center. The Fullerton Transportation Center is approximately 0.75 mile east of the project site. The project site increases the number of people who can conveniently access mass transit options, thereby increasing the potential ridership. Additionally, bike locker rooms are associated with Building 1 and Building 2.

The Fullerton Built Environment

Goal 7: Growth and development aligned with infrastructure capabilities.

Policy 7.2 – Housing Growth: Support projects, programs, policies and regulations to accommodate housing growth consistent with the Regional Housing Needs Assessment in areas of the City with existing and planned infrastructure capabilities.

The project's 295 dwelling units would support housing growth in the City and would help meet the City's future housing needs. The proposed dwelling units would be located on a site adjacent to residential uses and where existing infrastructure lines and capabilities are available to serve the project. No upgrades to off-site utilities are needed to serve the proposed project.

The Fullerton Built Environment

Goal 8: Protection from the adverse effects of noise.

Policy 8.3 Consideration of Noise in Land Use Decisions: Support projects, policies and regulations which ensure noise-compatible land use planning recognizing the relative importance of noise sources in order of community impact, the local attitudes towards these sources, and the suburban or urban characteristics of the environment, while identifying noise sensitive uses.

Policy 8.5 Focus Area Planning: Support projects, programs, policies and regulations to evaluate ways to ensure noise-compatible land use planning as part of the community-based planning of Focus Areas.

A noise study has been prepared to evaluate noise impacts from the development of the Specific Plan. The study has been incorporated into the Project's Mitigated Negative Declaration. All potential impacts can be mitigated to a less than significant level.

The Fullerton Community

Goal 12: Proactively addressing public safety concerns.

Policy 12.13 – Safety through Design: Support policies, projects, programs and regulations that make crime prevention and the maintenance of public safety service levels considerations in design and management of existing and new private and public spaces.

Safety considerations have been incorporated into the design of the proposed project. Site lighting would be used to provide adequate lighting for circulation, safety, and security. The project would also include two midblock traffic calming (raised) crosswalks and textured paving would be installed on Williamson Avenue to create a pedestrian zone between the two building lobbies and outdoor courtyard areas. The City reviews all new development proposals for sensitivity to crime prevention through site design, and the Specific Plan includes design guidelines related to safety and crime prevention.

The Fullerton Community

Goal 14: An environment with opportunities for community health and wellbeing.

Policy 14.5 Opportunities for Physical Activities: Support policies, programs and regulations that provide for convenient and safe areas that facilitate opportunities for physical activity such as parks, trails, open space, safe streets for bicycling, safe sidewalks for walking, and recreational facilities for residents of all ages and abilities.

Policy 14.6 Amenities Within a Walkable Distance: Support policies and regulations involving land use and zoning changes that would provide access to daily retail needs, recreational facilities, and transit stops within a walkable distance (i.e., a quarter- to a half-miles) of established residential uses.

The proposed project includes recreational and fitness facilities as part of the mixed-use development for the use by the residents. On-site bike lockers on the ground floor level of each parking structure would be provided, for a total of 70 bike lockers. The project site has access to the existing City sidewalk system and is located approximately 0.2 mile southwest of Ford Park and 0.25 mile west of Amerige Park.

The Fullerton Community

Goal 15: Parks, recreational facilities, trails, and programs that promote a healthy community and a desirable quality of life.

Policy 15.10 Park Dwelling Fee: Support policies and regulations which require new construction of dwelling units in the City to pay a park dwelling fee that provides for the creation and enhancement of open space, parks and recreational facilities accessible to all residents.

The Applicant/Developer of the project would pay the City's prescribed Park Dwelling fee to support the acquisition, development, and maintenance of the City's public park and trail system.

The Fullerton Natural Environment

Goal 20: A healthy watershed and clean urban runoff.

Policy 20.5 Water Quality of Focus Areas: Support projects, programs, policies and regulations to encourage site and infrastructure improvements within the City's Focus Areas to support cleaner and reduced urban runoff.

Policy 20.6 Construction Impacts: Support projects, programs, policies and regulations to reduce impacts to watersheds and urban runoff caused by private and public construction projects.

Policy 20.7 Development Impacts: Support projects, programs, policies and regulations to reduce impacts to watersheds and urban runoff caused by the design or operation of a site or use.

The existing site is presently 100 percent impervious. The proposed project would incorporate on-site storm water treatment for the property before the water leaves the site and enters the City's storm drain system. Additionally, the project would add pervious surfaces to the site, including landscaping. This would improve water quality on the site from its existing conditions.

The Fullerton Natural Environment

Goal 21: Protection and improvement of air quality.

Policy 21.6 Construction Impacts: Support projects, programs, policies and regulations to reduce impacts to air quality caused by private and public construction projects.

Policy 21.7 Development Impacts: Support projects, programs, policies and regulations to reduce impacts to air quality caused by the design and operation of a site or use.

The proposed project would be required to comply with all measures set forth to reduce construction and operational air quality impacts. The project would be consistent with the objectives for the Commonwealth Corridor and the required General Plan Amendment would be consistent with The Fullerton Plan.

1.5 PLAN ORGANIZATION

Section 1 – Introduction

This section explains the purpose of the Specific Plan; local and regional context and setting; background; planning process and entitlements; guiding principles; authority to prepare; relationship to existing plans and policies; and organization of the Specific Plan.

Section 2 – Development Plan and Development Regulations

This section explains the conceptual land use plan for the Specific Plan Area; identifies land use policies, and defines the land use designations unique to the Specific Plan. The circulation components of the plan will also be described. This section also explains the development standards for the land use designations established in the Development Plan, including the standards for residential, commercial, recreation, parking, and signage.

Section 3 – Urban Design

This section explains design concepts and establishes design guidelines for development in the Specific Plan Area.

Section 4 – Infrastructure and Services

This section explains the infrastructure requirements for the project, including drainage, water and sewer, and grading. Public services plans are also described.

Section 5 – Administration and Implementation

This section discusses the development review procedures by the City of Fullerton and other relevant permitting agencies, applicable to the Specific Plan Area. Implementation of the proposed land uses, including Specific Plan adoption, subsequent approvals and plans, and phasing are outlined in this chapter. Additionally, financing sources and maintenance responsibilities are identified.

2 **DEVELOPMENT PLAN AND LAND USE REGULATIONS**

This section explains the conceptual land use plan for the Specific Plan Area; identifies land use policies, and defines the land use designations unique to the Specific Plan. The circulation components of the plan will also be described. This section also explains the development standards for the land use designations established in the Development Plan, including the standards for residential, commercial, recreation, parking, and signage.

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2.1 LAND USE PLAN

The development plan envisions two residential structures totaling 295 multifamily units in a wrap configuration with internal parking structures, and up to 4,822 square feet of retail uses facing West Commonwealth Avenue. The plan represents a positive change from the site's underutilized development. The site plan, shown in Figure 2-1 *Site Plan*, provides the overall vision and guide for the development of the site. The plan is comprised of a single mixed-use district, divided into two parcels. These two parcels are legal lots from a previous subdivision:

- Parcel A. Parcel A is a 2.97-acre parcel located on the southwest corner of the Commonwealth Avenue/Chestnut Place intersection.
- Parcel B. Parcel B is a 1.82-acre parcel located on the southwest corner of the Williamson Avenue/Chestnut Place intersection.

Table 2-1, *Land Use*, presents a statistical summary of the project. The site plan (Figure 2-1, *Site Plan*), shows the overall plan for the comprehensive development program for the site.

Land Use	Acreage	Dwelling Units	Commercial (Sq. Ft.)	Floor Area Ratio (FAR)	Density (du/ac)
Parcel A/Building 1	2.97 ac.	172	4,822	2.71	57.9
Parcel B/Building 2	1.82 ac.	123	--	3.28	67.6
Total	4.79 ac.	295	4,822	2.93	61.6

Any elements not addressed in the Specific Plan would revert to the relevant provision in the Fullerton Municipal Code.

Land Use Categories

To implement the stated concepts and goals of this Specific Plan, land uses have been grouped into two land use categories:

Multifamily Residential. The project will contain up to 295 multifamily residential units.

Commercial. Up to 4,822 square feet of retail commercial is proposed in a mixed use setting. Uses will be primarily small scale retail and professional services meant to serve the daily convenience needs of the project's residents and nearby existing residential uses. This could include but is not limited to restaurant, coffee shop and bakery, convenience store, professional services (e.g., insurance, financial brokerage, etc.).

2.1.1 Multifamily Residential

Parcel A would include a four-story multi-family apartment building (Building 1) with 172 residential units and approximately 4,822 sf feet of ground floor retail space. The building would wrap around a five-level, aboveground parking structure.

Parcel B would include a four-story multi-family apartment building (Building 2) with 123 residential units. Amenities on Parcel B would include bike lockers, a fenced dog park, a garden courtyard, and club room. The building would wrap around a six-level, aboveground parking structure on three sides; no units would be located on the south side of the building which faces the railroad right-of-way.

Table 2-2 Residential Use				
Unit Type	Net Area (sf)	Building 1	Building 2	Total Units
Studio	550	4	8	12
1-Bedroom	750 – 866	85	65	150
2-Bedroom	1,075 – 1,086	67	50	117
3-Bedroom	1,275	16	0	16
Total	n/a	172	123	295

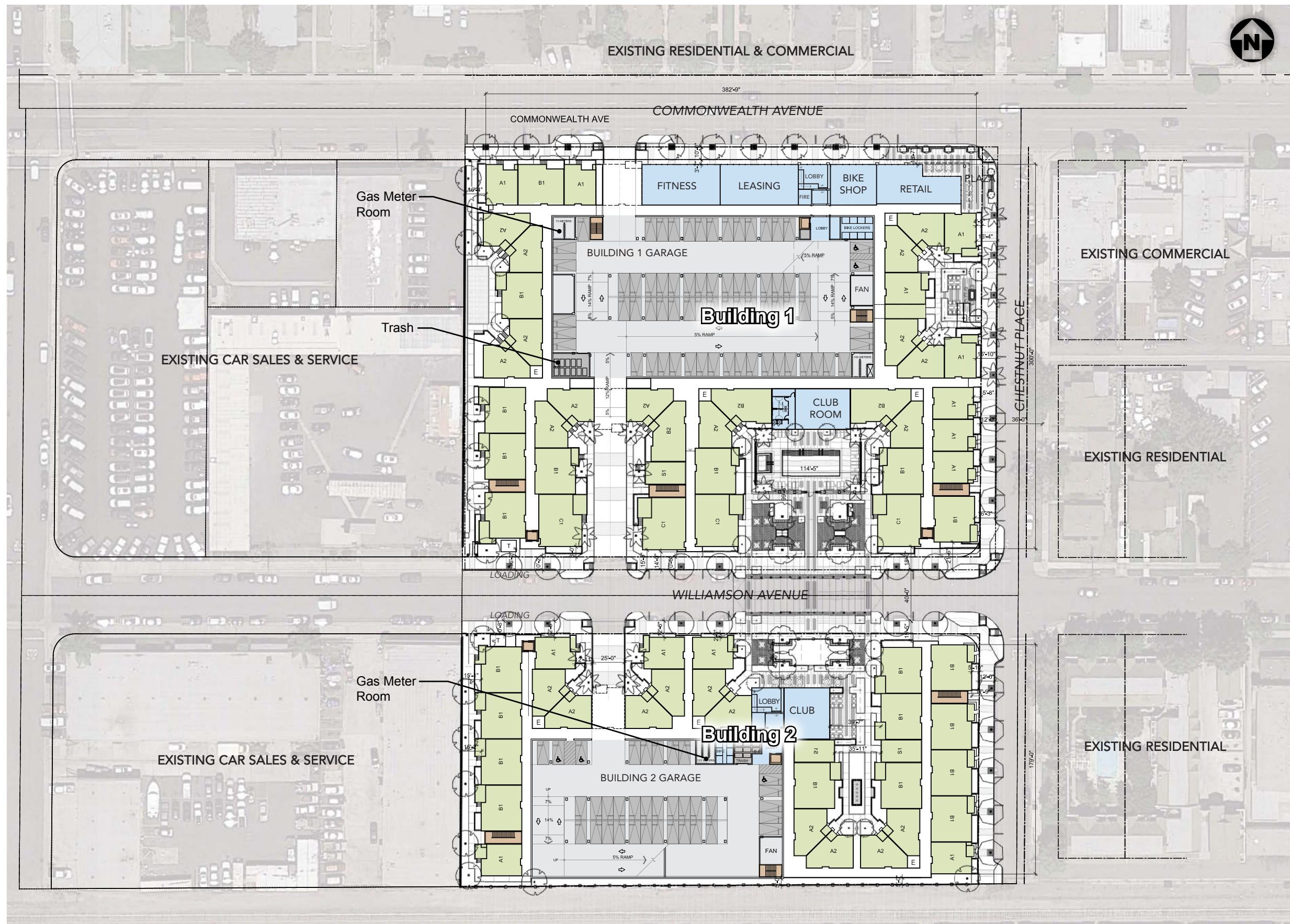
2.1.2 Commercial Uses

At maximum of 4,822 square feet of retail commercial uses are permitted within the Specific Plan area as shown in Table 2-1, *Land Use*. This retail commercial use shall be designed in a vertical mixed-use configuration and facing an arterial roadway. The commercial uses are limited to Parcel A/Building 1, facing West Commonwealth Avenue.

As an interim use, the retail space in Building 1 may be used for ancillary residential facilities, including a leasing office; fitness center for residents; bike shop for residents; as well as limited retail space. See Section 2.5.1, *Permitted Uses*, below for a listing of allowable uses.

2.1.3 Recreation and Amenities

As noted in the Development Standards in this section of the Specific Plan, a minimum of 15% of the lot must be allocated to common open space. In addition to common open space requirements, private open space of 50 square feet per unit is required in total. Where provided as balconies or patios, this private open space must have a minimum dimension of five feet. The private open space allocation may be combined all or in part to increase the common open space areas.



Source: TCA Architects

Figure 2-1: Conceptual Site Plan

NOTES

PRIVATE OPEN SPACE

(REQUIRED: 50 SF PER UNIT)

BUILDING 1 PARCEL




TOTAL UNITS	-	172
REQUIRED	-	8,600 SF
PROVIDED	-	9,230 SF

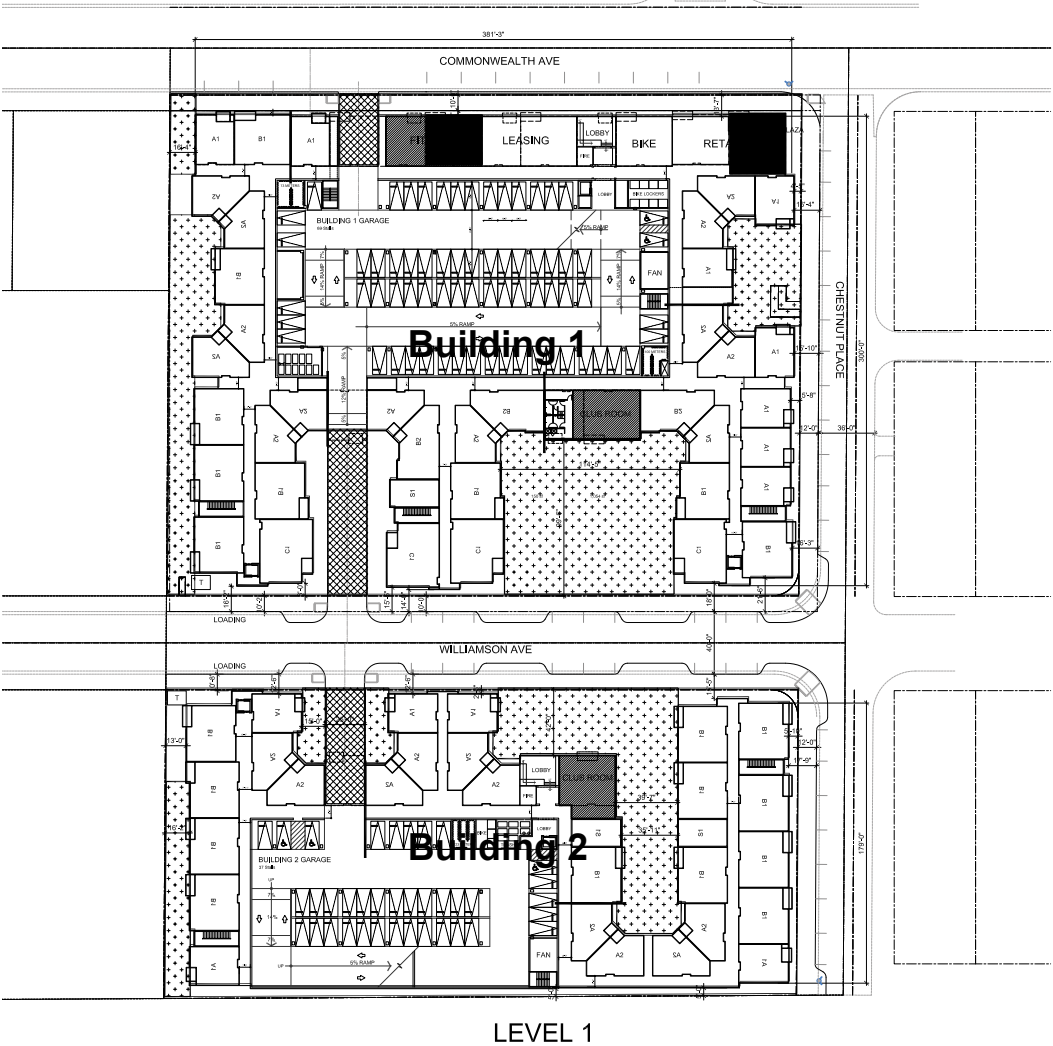
BUILDING 2 PARCEL

TOTAL UNITS	-	123
REQUIRED	-	6,150 SF
PROVIDED	-	6,250 SF

(Note: Studio Units do not contain any private open space. This lack of private open space has been substituted for additional Common Open space.)

KEY

	Open Space
	Roof Deck
	Driveway



NOTES

COMMON OPEN SPACE

(REQUIRED: 15% OF 208,552 SF (THE GROSS SQUARE FOOTAGE OF THE PROJECT AREA) = 31,283 SF

BUILDING 1 PARCEL

LOT SIZE	-	129,235 SF
DRIVEWAY	-	3,777 SF
OPEN SPACE	-	20,351 SF
DECK	-	2,555 SF

TOTAL OPEN SPACE PROVIDED	-	22,906 SF (18%)
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BUILDING 2 PARCEL

LOT SIZE	-	79,316 SF
DRIVEWAY	-	1,850 SF
OPEN SPACE	-	13,024 SF

TOTAL OPEN SPACE PROVIDED	-	13,024 SF (16%)
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AMENITIES

CLUBROOM	-	3,356 SF
BIKE KITCHEN	-	1,162 SF
FITNESS	-	1,961 SF

TOTAL OPEN SPACE PROVIDED	-	6,479 SF
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SITE TOTAL

BLDG 1 PARCEL	-	129,235 SF
BLDG 2 PARCEL	-	79,316 SF

TOTAL	-	208,552 SF
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BLDG 1 PARCEL OPEN SPACE PROVIDED	-	22,906 SF
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BLDG 2 PARCEL OPEN SPACE PROVIDED	-	13,024 SF
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TOTAL OPEN SPACE PROVIDED	-	35,930 SF (17%)
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Source: TCA

Figure 2-2: Open Space

As proposed, the courtyards are publicly accessible (i.e. ungated) except for the proposed pool and dog park area. Security fencing of these areas is permitted in the future if deemed necessary for safety or security reasons; addition of fencing and/or gates would require Community Development Director review and approval.

The project design includes a number of private and semi-public open spaces (see Figure 2-2, *Open Space*):

Building 1/Parcel A

North Plaza. This is a public plaza space on the corner of Commonwealth and Chestnut Place, located partially within the public street right-of-way. It is designed as a gathering space associated with the retail uses fronting on West Commonwealth. The design of this element includes landscaping and benches adjacent to the street frontage with trees and palms sheltering the seating from traffic. An overhead structure is planned as well as special railroad-inspired thematic paving. Final furnishings and design is subject to site plan review.

North Courtyard. This is a courtyard space which is part of Building 1, fronting on Chestnut Place. This private courtyard is a common open space area for use by residents, but is also publicly accessible. The design incorporates seating areas in an outdoor living-room format and a firepit. Final furnishings and design is subject to site plan review.

Building 2/Parcel B

Pool Courtyard. A plaza and courtyard space which is part of Building 1, fronting on Williamson Avenue. This courtyard area includes a fenced pool/spa for use by residents only. The courtyard may also include seating, game areas, barbeque areas, overhead structures and special railroad-inspired thematic paving, subject to site plan review. Thematic paving and speed table in Williamson Avenue visually connects this courtyard space with the courtyard associated with Building 2. Final furnishings and design is subject to site plan review.

South Courtyard. A plaza and courtyard space which is part of Building 2, fronting on Williamson Avenue. This courtyard area includes a fenced dog park for use by residents only. The courtyard also includes seating, water feature, overhead structures and special railroad-inspired thematic paving. Thematic paving in Williamson Avenue visually connects this courtyard space with the courtyard associated with Building 1. Final furnishings and design is subject to site plan review.

Roof Decks. Two roof decks are proposed on upper floors, for use by residents.

2.2 CIRCULATION PLAN

2.2.1 Regional Circulation

Regional access to the Specific Plan area is provided via West Commonwealth Avenue, Harbor Boulevard, Orangethorpe Avenue, and Euclid Street which connect with State Route (SR) 91 approximately one mile south, and SR-57, approximately three miles to the east. The Fullerton Amtrak/Metrolink Station, located less than one mile to the east, provides regional rail connections.

2.2.2 Arterials

Commonwealth Avenue is an east-west Primary Arterial Highway with two travel lanes in each direction. Commonwealth Avenue begins in Buena Park, extending east into the City of Fullerton and terminating at Chapman Avenue. On-street parking is permitted along the project frontage. The posted speed limit varies between 30 and 35 mph.

Project-related improvements to West Commonwealth Avenue are limited to construction of a project access driveway (see discussion below). Reconstruction of the curb and sidewalk, as well as provision of new street trees, street furniture, and a publicly accessible plaza space are required along this frontage (see discussion in the Urban Design section of this document).

2.2.3 Local Streets

Both Williamson Avenue and Chestnut Place are local roadways, with one travel lane in each direction, and existing on-street parking. Williamson Avenue is presently constructed with a paved width of 40 feet. Chestnut Place is presently constructed with a paved width of 36 feet.

- **Williamson Avenue.** Project-related improvements to Williamson Avenue are limited to construction of project access driveways (see discussion below), and construction of bulb-outs to provide a sense of protection for on-street parallel parking and enhance the pedestrian experience on the project frontage. Reconstruction of the curb and sidewalk, as well as provision of new street trees are required along this frontage (see discussion in the Urban Design section of this document). The ultimate design and construction of the bulb-outs will require Public Works Department review and concurrence as part of site plan approvals.

Special paving treatments are permitted on Williamson Avenue to visually connect the project's two buildings and their plaza spaces, provide for pedestrian crossings, and act as traffic calming along this roadway segment.

- **Chestnut Place.** Project-related improvements are limited to construction of bulb-outs to enhance the pedestrian experience on the project frontage. Bulb-outs may not be used at the intersection of Chestnut Place and West Commonwealth Avenue. Reconstruction of the curb and sidewalk, as well as provision of new street trees, are required along this frontage (see discussion in the Urban Design section of this document). The ultimate design and construction of the bulb-outs will require Public Works Department review and concurrence as part of site plan approvals.

2.2.4 Site Access

The project access will consist of a three un-signalized entrances: one on West Commonwealth Avenue, and two on Williamson Avenue. The two entrances on Williamson Avenue will be aligned, forming one four-legged un-signalized intersection. Each entrance will provide one travel lane in each direction, with stop-sign control on the project driveway approach. The on-site circulation will consist of two-lane residential streets providing access to the on-site parking garages.

Building 1. Building 1 will include two entries to the building's parking structure. Entries may be gated, subject to site plan review.

- The entrance on West Commonwealth Avenue provides access to a five-level parking structure for the 172 apartment units and the retail uses. A portion of the parking stalls on the first level will be reserved for the non-residential uses on the ground floor facing Commonwealth Avenue.
- The entrance on Williamson Avenue will also provide access to the five-level parking structure and will serve as the primary entry/exit point for residents of the building.

Building 2. Building 2 will provide a single entry to its parking structure. The entrance on Williamson Avenue will provide access to the 123 apartment units. This entrance will provide access to a six-level parking structure. Entries may be gated, subject to site plan review.

2.2.5 Transit

Public transit service in the project vicinity is provided by the Orange County Transportation Authority (OCTA). OCTA bus stops are currently located on the northwest corner of Drake Avenue at Commonwealth Avenue, and the southeast corner of Woods Avenue at Commonwealth Avenue.

OCTA Route 26 operates between the Cities of Fullerton and Yorba Linda via Commonwealth Avenue and Yorba Linda Avenue in an eastbound and westbound direction and operates seven days a week, with 30-minute headways throughout the day.

2.2.6 Parking

Parking for the project is provided in two parking structures, wrapped by the residential buildings. A portion of the parking stalls on the first level of Building 1 will be reserved for the retail uses.

On-street parking is presently provided on the streets adjacent to the project site and will continue to be provided on West Commonwealth Avenue, Chestnut Place, and Williamson Avenue. The circulation plan includes corner bulb-outs on Williamson Avenue and portions of Chestnut Place to act as traffic calming devices; this design feature will be reviewed by Fullerton's Public Works Department as part of site plan review.

Parking Standards shall be defined as outlined in Section 2.6, *Parking*, below.

2.3 GENERAL PROVISIONS

The California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to cities to utilize Specific Plans for purposes of implementing the goals and policies of the City's General Plan.

This Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan area.

This section has been prepared in accordance with California Government Code Section 65450, et seq. and the City of Fullerton Zoning Ordinance (Title 15 of the Municipal Code). Regulations are proposed for residential and commercial uses. Application of these regulations is specifically intended to provide the most appropriate use of the land, create a harmonious relationship among land uses and protect the health, safety and welfare of the community. The following General Development Standards apply to all uses within the Specific Plan area.

2.3.1 Applicability

The 600 West Commonwealth Specific Plan has been developed as both a regulatory and land use policy document, which, upon adoption by ordinance will constitute the zoning for the property. Development plans or agreements, parcel maps, site plans or any other action requiring ministerial or discretionary approval of the subject property must be consistent with the Specific Plan. California Government Code, Section 65454 requires that a Specific Plan be consistent with the General Plan. Upon adoption, actions deemed to be consistent with the Specific Plan shall be judged to be consistent with the City of Fullerton General Plan.

Where conflicts exist between the standards contained in this Specific Plan and those found in the City of Fullerton General Plan, Zoning Ordinance or Municipal Code, the regulations and standards in this Specific Plan shall take precedence. Any area of site development, administration, review procedures, environmental review, landscaping requirements, and regulations not expressly addressed by this Specific Plan document shall be subject to the provisions of the City of Fullerton Municipal code or General Plan, using the context and objectives of this Specific Plan as a guide.

2.3.2 Severability

In the event that any regulation, condition, program, portion or policy of this Specific Plan or the application thereof to any person or circumstance is held to be invalid or unconstitutional by any court of competent jurisdiction, such portions shall be deemed separate, distinct and independent provisions and shall not affect the validity of the remaining provisions of this Specific Plan or applications thereof which can be implemented without the invalid provision or application.

2.3.3 Determination of Unlisted Uses/Interpretation

Any land use proposal not specifically covered by the provisions contained herein shall be subject to determination by the Community Development Director, with appeals to the Planning Commission in accordance with Title 15.56.020 of the Municipal Code.

When used in this document, the words “shall,” “will,” “is to,” and “are to” are always mandatory. “Should” is not mandatory but is strongly recommended; and “may” is permissive. The present tense includes the past and future tenses; and the future tense includes the present. The singular number includes the plural number, and the plural the singular, unless the natural construction of the word indicates otherwise. The words “include,” “includes,” and “including” mean “including but not limited to”.

2.3.4 Definitions

Unless otherwise specified below, terms used in this document shall have the same definitions provided in the City of Fullerton Municipal Code Title 15.04.040, “Interpretation and Definitions.” For use in this Specific Plan the following definitions apply:

Hybrid Court. The hybrid courtyard building type is composed of two building types: the stacked dwelling and courtyard housing arranged around a courtyard. Stacked dwellings are comprised of single-floor or multi-floor attached dwellings of similar configuration that are stacked either above or below. Courtyard

building types consist of residences that may be comprised of townhouses or flats, either singly or in combination with each other. The structures are arranged around one or more courts that are partly or wholly open to the street.

2.3.5 Use of Design Guidelines

Residential and commercial development shall be designed and built in substantial conformance with the Design Guidelines contained in this document (Refer to Section 5, *Design Guidelines*).

2.3.6 Subsequent Building Modification

Subsequent building modification, including additions and/or projections into setback areas permitted by the Specific Plan (subject to approval of standard City permits), shall match the architectural style of the primary structure and shall be constructed of the same materials and colors as the primary structure and/or in context with the overall Design Guidelines.

2.3.7 Projections into Required Yards

Projections into required setbacks are permitted for balconies, porches, and stoops, subject to site plan review.

2.3.8 Height

- Building height shall be measured from the adjacent finished grade to the roof parapet for residential structures. Rooflines must be appropriate to the architectural style.
- Architectural features such as, but not limited to, weather vanes, stair towers, elevator towers, mechanical equipment screening, clock towers, etc. appropriate to the architectural style of the structure may extend in height above the stated Building Height of the Primary Structure, subject to site plan review.

2.4 RESIDENTIAL USES

The development standards and product types included herein are intended to establish the minimum design parameters. The residential product type assumed within the project is a multi-family residential uses.

Multi-Family flats are attached multi-family homes with entries from common open space at the ground level or internal corridors for upper floors. Automobile access is via an alley or private drive. Resident parking spaces are provided in garages or designated on-site parking spaces, and guest parking spaces are provided in designated parking areas. The Residential category includes multi-family flats in a mixed use setting.

These development standards should be used in conjunction with Chapter 3, *Design Guidelines*, which describe and illustrate building designs, concepts, and features that will promote the high-quality development that is envisioned for the Specific Plan area.

2.4.1 Permitted Uses

- a. Multi-family residences (rental or ownership). Residential uses must be located behind or above ground floor non-residential/commercial uses when located on West Commonwealth Avenue, a primary arterial. Allowable building types include Stacked Dwellings and Hybrid Court housing types as defined in Section 2.2.2 above;
- b. Ancillary structures and uses, including recreation rooms, business centers, swimming pools/spas, barbeque areas, bicycle maintenance areas, and other amenities typically associated with a rental community for use by project residents. Security gating is permitted subject to site plan review;
- c. Management offices;
- d. Parking areas and structures in conjunction with a multi-family residential use.
- e. Home occupations, subject to the provisions of the Municipal Code;

2.4.2 Uses Subject to a Conditional Use Permit

- a. Licensed child-care centers.
- b. Wireless Communications Facilities subject to the provisions of Title 15 of Fullerton Municipal Code.

2.4.3 Development Standards

The development standards outlined in Table 5-1, *Development Standards*, apply to residential uses within the Specific Plan area. Where development standards are not described, the standards as described in the Fullerton Municipal Code shall apply. Standards stated are minimums unless otherwise indicated.

- a. Building shall be composed of 2 to 4-story volumes.
- b. A minimum of 90% of ground floor units shall have direct access to their entry doors from either the street or courtyard, regardless of the location of the unit.
- c. Access to each unit above the second level in the stacked dwelling element may be through an interior, double-loaded corridor of at least 6 feet in width with recessed doors.
- d. Entrance to the residential portions of the stacked dwelling element may be through a dedicated street-level lobby, or through a dedicated lobby accessible from the street or through a side yard. Controlled access is permitted; however, street-level gates are not allowed.

2.5 COMMERCIAL USES

The retail commercial land use provides locations for businesses that meet the day-to-day shopping and service needs of the residential use both within the project and for surrounding uses. Uses provide an array of small-scale, local serving retail and service commercial uses, such as cleaners, food services etc.

Locational Criteria: Commercial uses shall be limited to ground floor locations fronting on a Primary Arterial (West Commonwealth Avenue).

Interim Uses: Interim uses of commercial spaces which may include uses ancillary to the project's residential units, including but not limited to: bicycle maintenance facility, business center, leasing office, fitness room, meeting rooms.

2.5.1 Permitted Uses

- a. Eating Places, including restaurants, cafes, convenience foods and specialty foods;
- b. Food and Beverage Stores, including Bakeries, candy, nut and confectionery stores, convenience markets, small scale grocery;
- c. Retail Commercial uses, limited to local-serving commercial uses;
- d. Office;
- e. Personal Services such as Barber/Beauty shop, Laundry and Dry Cleaning Services, Locksmith, Pet Grooming, Photographic Studios, Picture Framing, Shoe Repair, Tailors/Dress Makers;
- f. Tutoring Center; and
- g. Other similar uses deemed compatible by the Community Development Director and permitted in the G-C zoning district.

2.5.2 Development Standards

The development standards outlined in Table 5-1, *Development Standards*, apply to all buildings/uses, including parking structures (as applicable), within the Specific Plan area. Where development standards are not described, the standards as described in the Fullerton Municipal Code shall apply. Standards stated are minimums unless otherwise indicated.

Table 5-1 Development Standards	
Item	Standard
Floor Area Ratio (max)	3.0*
Density (maximum)	62 du/ac*
Lot Width/Depth (minimum)	200ft/250ft
Arterial Street Setback (West Commonwealth)	0 ft.
Local Street Setback (front) (Williamson)	0 ft.
Local Street Setback (side)	5 ft.
Interior Side Setback	0 ft.
Rear Setback	
Ground Floor (Local Street)	15 ft
Upper Floors (Local Street)	20 ft
Railroad Right-of-Way	5 ft
Distance Between Buildings	10 ft.
Building Height (Stories - max)	
Mixed Use Structure	4 stories
Residential Structure	4 stories
Parking Structure	6 levels**
Retail Space floor height (min)	15 ft.
Upper Floor Height (min)	9 ft.
Open Space	
Private (patio or balcony, 5 ft min. dimension)	50 sf/unit***
Common	15% of Lot

*Density and Floor Area Ratio may be averaged across the entire Specific Plan Area

** When wrapped by a residential or mixed-use building.

*** Where common open space is provided in excess of 15%, the additional common open space may be counted towards the private open space requirement.

2.6 PARKING

- a. Required parking shall be accommodated in an underground garage, podium or wrapped garage, tuck-under parking, or a combination thereof.
- b. Where structured parking takes access from the street, it shall be designed with separate pedestrian access ways and the entry shall be recessed from the street.
- c. Parking within the Specific Plan area shall conform to the following parking ratios:

Residential Structured Parking (minimum)

Studio	1.0 per unit
1 Bedroom	1.5 per unit
2+ Bedroom	2.0 per unit
Guest Parking	0.15 per unit
Overall Combined Rate:	1.85 per unit.
Non-Residential:	4 spaces/1,000 sf

Accessible parking and loading requirements shall be governed by code.

2.7 SIGNAGE

Signage within the Specific Plan area shall conform to the requirements of the Fullerton Municipal Code Chapter 15.49.

3

URBAN DESIGN

This section explains design concepts and establishes design guidelines for development in the Specific Plan Area

This section contains the landscape and architectural design guidelines for the 600 West Commonwealth Specific Plan area. These elements, when implemented, will ensure that the Specific Plan area develops as a quality mixed use community with consistent design elements. The design elements herein should be used in conjunction with the development standards described in the project's Development Regulations (Chapter 2).

The essence of good design includes both creativity and flexibility. These design features are intended to foster these ideals and promote innovative design solutions.

3.1 ARCHITECTURE

The intent of the architectural guidelines is to provide a basis for overall design cohesiveness, and to ensure a standard of architecture that reflects a pleasing style, quality materials and professional workmanship. Figures 3-1 through 3-8, *Building Elevations*, provide an illustration of the elevation treatments for residential buildings and commercial frontages.

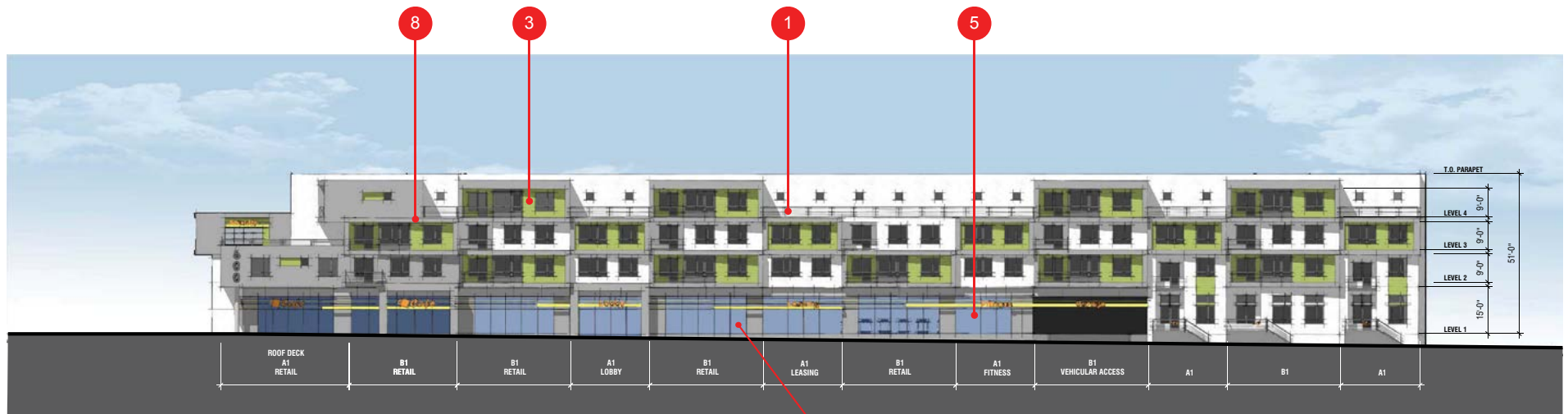
3.1.1 Architectural Design Inspiration

The project's architectural design inspiration was drawn from the City's rail history and the project's location adjacent to the railroad tracks.

Both the adjacent streets and railroad tracks for the design inspiration for the project based on the theme of motion/locomotion. The project's Commonwealth edge creates movement within the roof line by removing dwellings from the upper floors. This movement began a pattern which is continued along the facade through a series of "box car" like frames. Their alternating pattern along the façade creates "motion" much like the moving wheels of a locomotive. The retail and amenity base to the building, with its storefront glass and linear signage, acts as the train tracks for the building to move along.

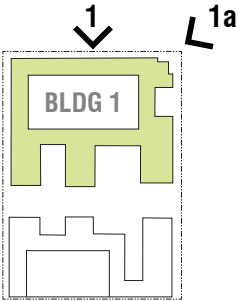
The edge along the train tracks requires special consideration. There was a need to protect the site from the sound of the train, while at the same time celebrate the fact that the proposed buildings are inspired by motion. The solution was to place the parking garage along the tracks to act as a buffer, while creating an architectural expression on the garage that followed the movement of the automobile along the garage ramp, exposing the movement of cars within the structure. This edge has also incorporated a painted mural depicting boxcars commonly seen travelling on the adjacent railroad tracks.

Figures 3-1 through 3-8 illustrate the building elevations for Buildings 1 and 2. Figure 3-9 is an illustrative graphic of the project.



1. BUILDING 1 - FRONT ELEVATION - COMMONWEALTH AVE

Transparent shopfront windows



MATERIAL LEGEND

1. Metal Balcony Railing
2. Fiber Cement Board - Wood
3. Fiber Cement Board - Color
4. Residential Wall Light - A
5. Aluminum Storefront
6. Metal Awning
7. Residential Wall Light - B
8. Exterior Plaster
9. Vinyl Window
10. Chainlink Mesh



1a. BUILDING 1 - PERSPECTIVE VIEW - COMMONWEALTH AVE

Source: TCA

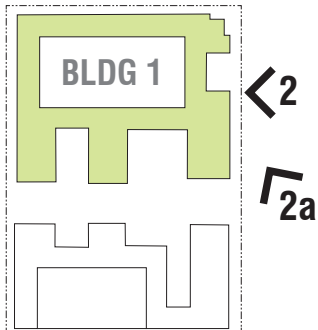
Figure 3-1: Building 1 Elevation



2. BUILDING 1 - LEFT ELEVATION - CHESTNUT PLACE



2a. BUILDING 1 - PERSPECTIVE VIEW - CHESTNUT PLACE

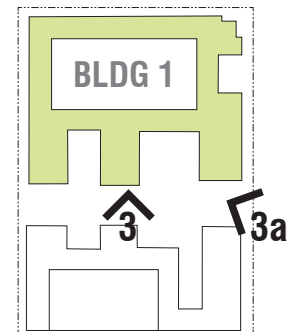


Source: TCA Architects

Figure 3-2: Building 1 Elevations



3. BUILDING 1 - REAR ELEVATION - WILLIAMSON AVE

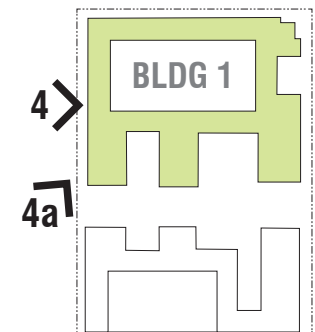


3a. BUILDING 1 - PERSPECTIVE - WILLIAMSON AVE

Source: TCA Architects



4. BUILDING 1 - RIGHT ELEVATION - PROPERTY LINE



4a. BUILDING 1 - PERSPECTIVE - RIGHT ELEVATION

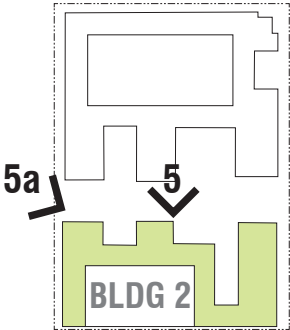
Source: TCA Architects



5. BUILDING 2 - FRONT ELEVATION - WILLIAMSON AVE



5a. BUILDING 2 - PERSPECTIVE - WILLIAMSON AVE



Source: TCA Architects

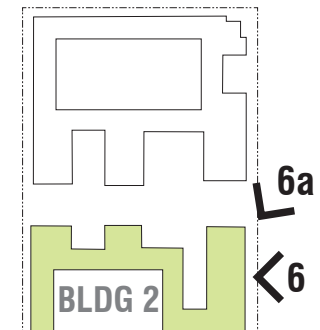
Figure 3-5: Building 2 Elevations



6. BUILDING 2 - LEFT ELEVATION - CHESTNUT PLACE



6a. BUILDING 2 - PERSPECTIVE - CHESTNUT PLACE



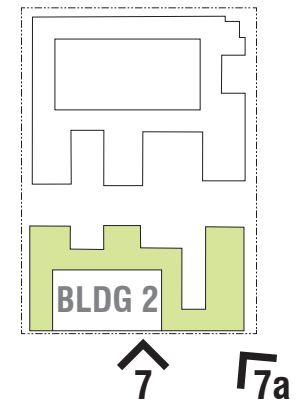
Source: TCA Architects



7. BUILDING 2 - REAR ELEVATION - RAILROAD TRACKS



7a. BUILDING 2 - PERSPECTIVE - REAR ELEVATION



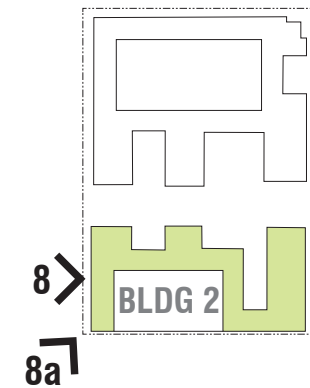
Source: TCA Architects



8. BUILDING 2 - RIGHT ELEVATION - PROPERTY LINE



8. BUILDING 2 - PERSPECTIVE - RIGHT ELEVATION



Source: TCA Architects



Source: TCA Architects

Figure 3-9: Artistic Rendering

3.1.2 Mass and Form

The street is often described as an outdoor room with its walls defined by the façades of its buildings. How building massing is distributed on a site usually has the greatest impact on a project's overall appearance.

- At the corner of West Commonwealth Avenue and Chestnut Place, the building massing is intentionally dropped to two stories to respond to the neighbors across the street and bring the building down to a human scale where interaction can take place at the retail base and roof deck above.
- The Williamson elevations transition into a more residential scale and character. The building is oriented to bring minimal building mass to the street and maximize the courtyards and greenbelts, much like a front lawn of a home. Stoops with front door access enhance the neighborhood feel along this street. The two buildings are divided by Williamson, but the courtyards relate to each other by expanding their footprint across the street, accomplished by enhancing the paving on Williamson to reflect that which is used in the courtyards, creating a visual connection between the buildings.
- Chestnut joins Commonwealth and Williamson together. The architectural expressions of locomotion and residential combine here in a three story expression. This three story edge helps in the transition to neighbors to the West.
- Buildings shall be no higher than four stories wrapped around structured parking.
- Flat roofs shall provide parapets or other architecturally integrated methods to screen roof top equipment. The design and color shall be integral with the appearance of the building.
- Roof heights shall vary to encourage an interesting street scene.
- Orient buildings around courtyards; preferably courtyards that are open to the street.

3.1.3 Façade Articulation

Articulation is the portions of a building façade that is differentiated from a building envelope by elements that project or are recessed from the primary façade.

- Façade articulation should incorporate the use of balconies, overhangs and changes in plane to create movement on all sides of a building.

3.1.4 Materials and Colors

A variety of high-quality, durable colors must be provided to create interesting and attractive building designs and avoid monotony.

- Building materials shall convey a sense of permanence and complement a building's architectural style. Materials shall be durable, authentic and applied with consistency. Material changes at outside corners shall be avoided, as shall any application appearing as glued on. Facade elements constructed of foam or foam molding are prohibited.



- Exterior colors and materials should be used to define building form, details, and massing.
- Brick, natural stone, precast concrete, and factory-finished metal panels (heavy gauge only) are preferred.
- Alternatives to stucco are preferred. When stucco is used, seams shall be incorporated to create visual interest and the aggregate mix of the stucco shall be no less than 20/30 with a sand float finish or smooth steel-trowel finish.
- The finish, texture, and color of materials, including windows, shall be compatible with the overall architectural theme.
- Windows shall be of high quality and afford shadow line and depth. This may be achieved through inset windows with an integral frame or recessing the window into the exterior wall. Windows installed flush with the exterior building material are prohibited.
- Materials and colors used on the front facade shall be wrapped along the side facade to an appropriate transition point several feet beyond the front elevation to avoid the appearance of false facades. Bright accent colors may be used for contrast, subject to review and approval by the Community Development Director. Materials may include metal awnings, fiber cement panels, and aluminum storefront windows for retail uses. Chain link mesh is an allowable material for parking structure wall materials facing the railroad frontage only.
- Decorative elements shall be used to break up the plane of the facade and create visual interest. Encouraged decorative elements include murals, metal awnings and other similar elements appropriate for the architectural style of the building.
- Building details such as flashing, pipes, and metal vents may be used as an enhancement with complementary colors/materials or painted to match the building or roof surface.
- Low and no volatile organic compound (VOC) paint and finishes are required.



3.1.5 Safety Through Design

Resident safety can be enhanced through the application of design considerations that contribute to the reduction in opportunities for crime. The following guidelines apply to the Specific Plan area:

- Open spaces, plazas, circulation corridors, and individual living unit entries should be designed to be as visible from as many dwelling units as possible. Site entries should be visible from a street or driveway.
- On-site lighting should be sufficient for sidewalk and internal driveway illumination. Pedestrian scale lighting fixtures are encouraged.

- Use natural surveillance (i.e. “eyes on the street”) to maximize the visibility of people, parking, and building entrances.
- Windows and entries should be placed to maximize surveillance. Sight lines from dwelling units to parking areas should be provided and maintained.
- The management and/or rental offices, recreation rooms, and recreational amenities should be located in a central, visible location, close to heavily used areas.

3.2 LANDSCAPE ARCHITECTURE

3.2.1 Landscape Master Plan

Landscape treatments around buildings will be designed to help break up the building massing by incorporating both tall, vertical trees and lower growing and broader canopy trees along West Commonwealth Avenue. The ground plane will be landscaped with a mix of shrubs and ground cover plants to create a layered appearance. Recreation areas will include a variety of shrub and tree plantings to provide vertical accents and shade near the amenities. A Conceptual Landscape Plan for the Specific Plan area is illustrated on Figures 3-10, -11, and -12 *Landscape Master Plan* and *Plant Materials*. Trees used within the Specific Plan will be selected from the list on Figures 3-11 and 3-12, *Plant Materials*. Shrubs and groundcovers will be selected concurrent with final designs for individual projects within the Specific Plan area.

- Landscaping shall be provided in all setback areas of the Specific Plan area.
- The Specific Plan area shall comply with the landscape design measures to reduce water use contained within Section 15.50 of the Municipal Code.
- Streetscapes will incorporate low water use plant materials to minimize irrigation needs. Open space areas not planted with living material should utilize permeable materials such as decomposed granite, mulch and/or rocks/cobble to reduce irrigation demands where possible.
- The community shall be irrigated with reclaimed or recycled water if available.
- Planting areas will be irrigated with a high efficiency automatic irrigation system.

3.2.2 On-site Gathering Spaces

The provision of on-site open spaces such as plazas, courtyards, and outdoor dining plazas are integral components of a pedestrian oriented development. These private and semi-public spaces provide a finer-grained, more intimate setting that encourages pedestrians to gather and linger, and can be designed to complement and enhance the commercial and residential uses.

The Landscape Master Plan illustrates a number of amenity areas within the plan. Figure 3-10, *Landscape Master Plan*, identifies these areas.

- Common open space provides opportunities for social interaction and safe play areas for children, and reduces the perceived density of the development. Private open space serves as an outdoor room for residents.

- Residents should have access to useable open space for recreation and social activities. Open spaces should be conveniently located for the majority of units.

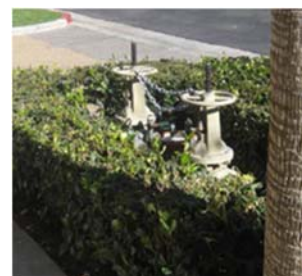
The project design includes a number of private and semi-public open spaces:

- **North Plaza.** A plaza space on the corner of Commonwealth Avenue and Chestnut Place, designed as a gathering space associated with the retail uses fronting on West Commonwealth. The design of this element includes landscaping and benches adjacent to the street frontage with trees and palms sheltering the seating from traffic. An overhead structure is planned as well as special railroad-inspired thematic paving. Figure 3-13, *Plazas and Open Spaces*, illustrates this publicly accessible plaza area.
- **North Courtyard.** A plaza and courtyard space which is part of Building 1, fronting on Chestnut Place. This private courtyard is a common open space area for use by residents, but is also publicly accessible. The design incorporates seating areas in an outdoor living-room format and a firepit. Figure 3-13, *Plazas and Open Spaces*, illustrates this publicly accessible plaza area.
- **Pool Courtyard.** A plaza and courtyard space which is part of Building 1, fronting on Williamson Avenue. This courtyard area includes a fenced pool/spa for use by residents only. The courtyard also includes seating, game areas, barbeque areas, overhead structures and special railroad-inspired thematic paving. Figure 3-14, *Pool Courtyard*, illustrates this publicly accessible courtyard area. Thematic paving and speed table in Williamson Avenue visually connects this courtyard space with the courtyard associated with Building 2.
- **South Courtyard.** A plaza and courtyard space which is part of Building 2, fronting on Williamson Avenue. This courtyard area includes a fenced dog park for use by residents only. The courtyard also includes seating, water feature, overhead structures and special railroad-inspired thematic paving. Figure 3-15, *South Courtyard*, illustrates this publicly accessible courtyard area. Thematic paving in Williamson Avenue visually connects this courtyard space with the courtyard associated with Building 1.
- **Roof Decks.** Two private roof decks are proposed on upper floors for use by residents.

3.2.3 Utility Placement and Screening

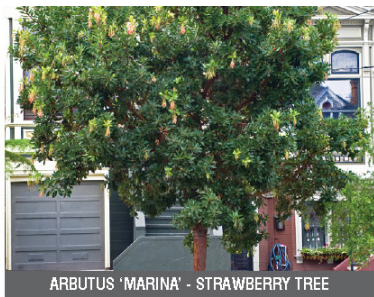
Utility boxes should be grouped where possible and placed in landscape setbacks and/or shrub/groundcover areas.

Above grade utility boxes should be screened and planted to the extent possible while allowing required access and clearance, and providing for adequate sight distance if located near intersections.

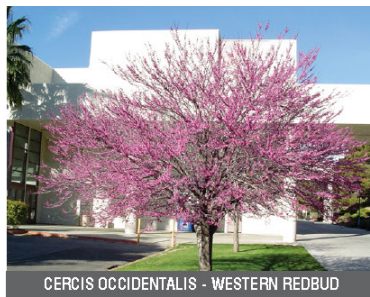


3.3 Walls and Fences

The project has limited walls, limited in location to the western edge with adjoining commercial property, required pool/spa fencing for code and access reasons, and the private dog park area in the south courtyard areas. Figure 3-16, *Fencing Plan*, illustrates the fencing locations and type.



ARBUTUS 'MARINA' - STRAWBERRY TREE



CERCIS OCCIDENTALIS - WESTERN REDBUD



CHAMAEROPS HUMILIS - MEDITERRANEAN FAN PALM



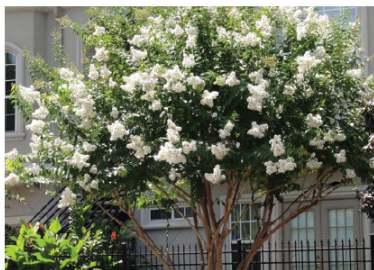
DRACAENA DRACO - DRAGON TREE



GLEDITSIA TRIACANTHOS - HONEY LOCUST



KOELREUTERIA BIPINNATA - CHINESE FLAME TREE



LAGERSTROEMIA INDICA - CRAPE MYRTLE



LIQUIDAMBAR STYRACIFLUA



MAGNOLIA GRANDIFLORA - SOUTHERN MAGNOLIA



MELALEUCA NESOPHILA - PINK MELALEUCA



PARKINSONIA FLORIDA - BLUE PALO VERDE



PHOENIX DACTYLIFERA - DATE PALM



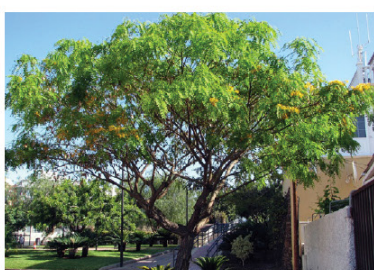
PISTACHIA CHINENSE - CHINESE PISTACHE



PLATANUS X HISPANICA - LONDON PLANE TREE



PYRUS CALLERYANA - FLOWERING PEAR



TIPUANA TIPU - TIPU TREE



TRISTANIA CONFERTA - BRISBANE BOX



WASHINGTONIA ROBUSTA - MEXICAN FAN PALM

Source: Land Studio 360

Figure 3-11: Plant Materials - Trees



Source: Land Studio 360

CONCEPTUAL PLANT LIST

*Plant Factor per WUCOLS (water use classification of landscape species): VL=Very Low L=Low M=Moderate H=High		
#	PLANT NAME	PF*
1	Agave spp. - Agave	L
2	Bambusa spp. - Bamboo	M
3	Bougainvillea spp. - Bougainvillea	L
4	Bulbine frutescens - Orange Stalked Bulbine	L
5	Carex pansa - Dune sedge	M
6	Chondropetalum tectorum - Cape Rush	L
7	Coprosma spp. - Mirror Plant	M
8	Dianella spp. - Flax Lily	M
9	Distictis buccinatoria - Blood Red Trumpet Vine	M
10	Echeveria spp - Echeveria	L
11	Elaeocarpus dicipiens - Japanese Blueberry	M
12	Encelia californica - Coast Sunflower	VL
13	Ipomoea batatas 'Marguerite' - Sweet Potato Vine	L
14	Lavatera assurgentiflora - Island Tree Mallow	L
15	Leucadendron spp. - Willow Cone Bush	L
16	Ligustrum japonicum 'Texanum' - Texas Privet	M
17	Lonicera japonica 'Halliana' - Hall's Honeysuckle	L
18	Lotus maculatus - Parrot's Beak	M
19	Muhlenbergia capillaris - Pink Muhly Grass	M
20	Ophiopogon planiscarpus 'Nigrescens' - Black Mondo Grass	M
21	Phormium spp. - New Zealand Flax	L
22	Phyllostachys nigra - Black Bamboo	M
23	Pittosporum tenuifolium - Kohuhu	M
24	Rhamnus californica - Coffeeberry	VL
25	Rosmarinus officianalis - Rosemary	VL
26	Salvia spp. - Sage	L
27	Senecio mandraliscae - Blue chalk fingers	L
28	Senna artemisioides - Feathery Cassia	L
29	Umbellularia californica - California Laurel	L
30	Zauschneria californica - California Fuchsia	VL

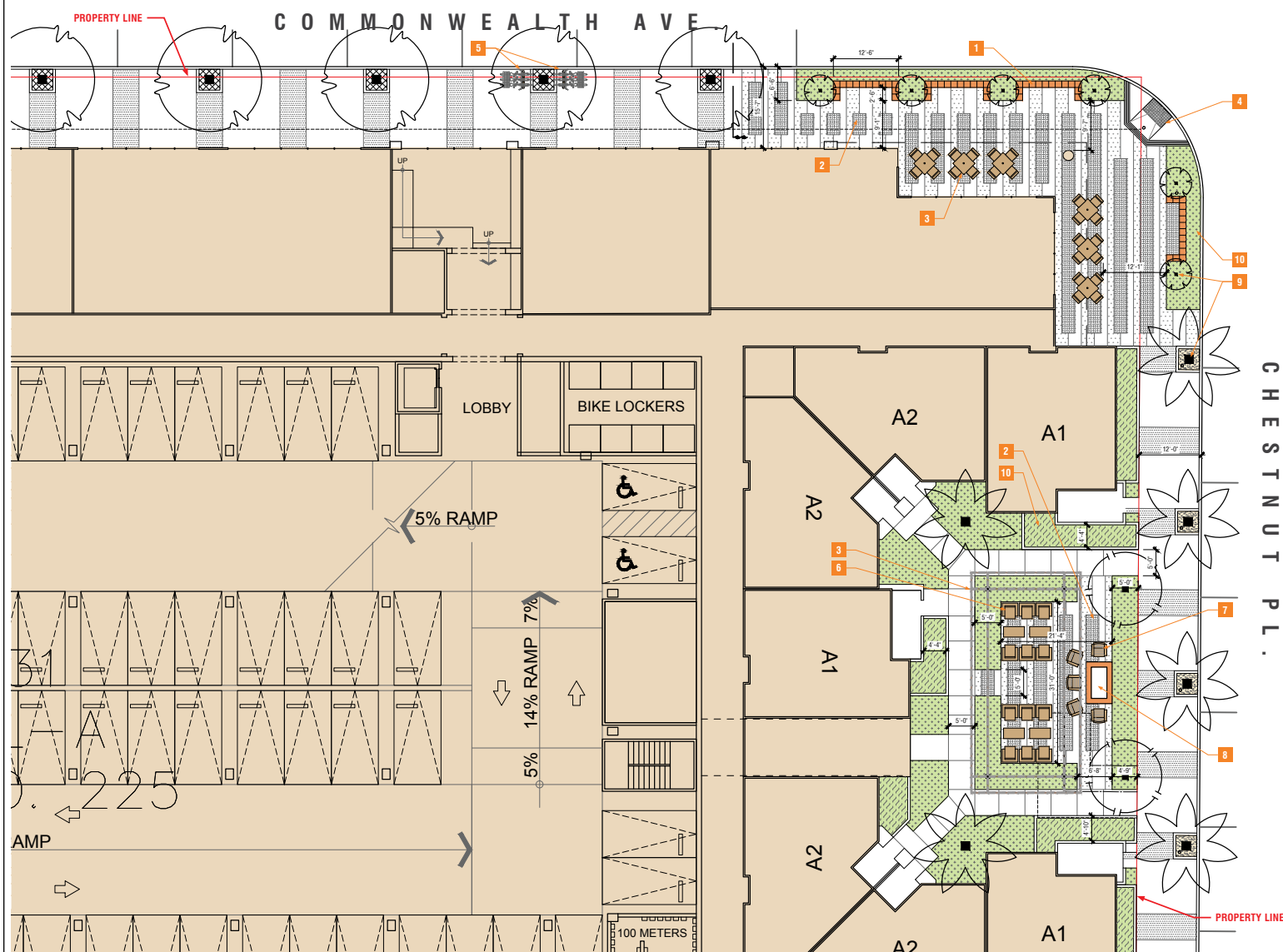
ESTIMATED WATER BUDGET

Reference ETo	Irrigation Efficiency (IE)	Max Applied Water Allowed
50.22	0.81 (Drip Irrigation)	276,991 gal./yr.
HZ#1	VL = 0.1	13% of site planter area
		9,879 gal./yr.
HZ#2	L = 0.3	47% of site planter area
		107,149 gal./yr.
HZ#3	M = 0.5	40% of site planter area
		151,984 gal./yr.
Total Estimated Water Use = 269,012 gal./yr.		

GENERAL NOTES

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Figure 3-12: Plant Materials



LEGEND

- 1 Thematic board-formed concrete seatwalls
- 2 Railroad-inspired thematic paving within plaza and courtyard
- 3 Dining seating for cafe
- 4 Curb ramp
- 5 Bike racks
- 6 Lounge furnishings and ADA accessible seating
- 7 Adirondack-style chairs
- 8 Fire pit
- 9 Trees and Palms - see sheet L-8
- 10 Shrub and groundcover planting areas - plant list on sheet L-9

KEYMAP

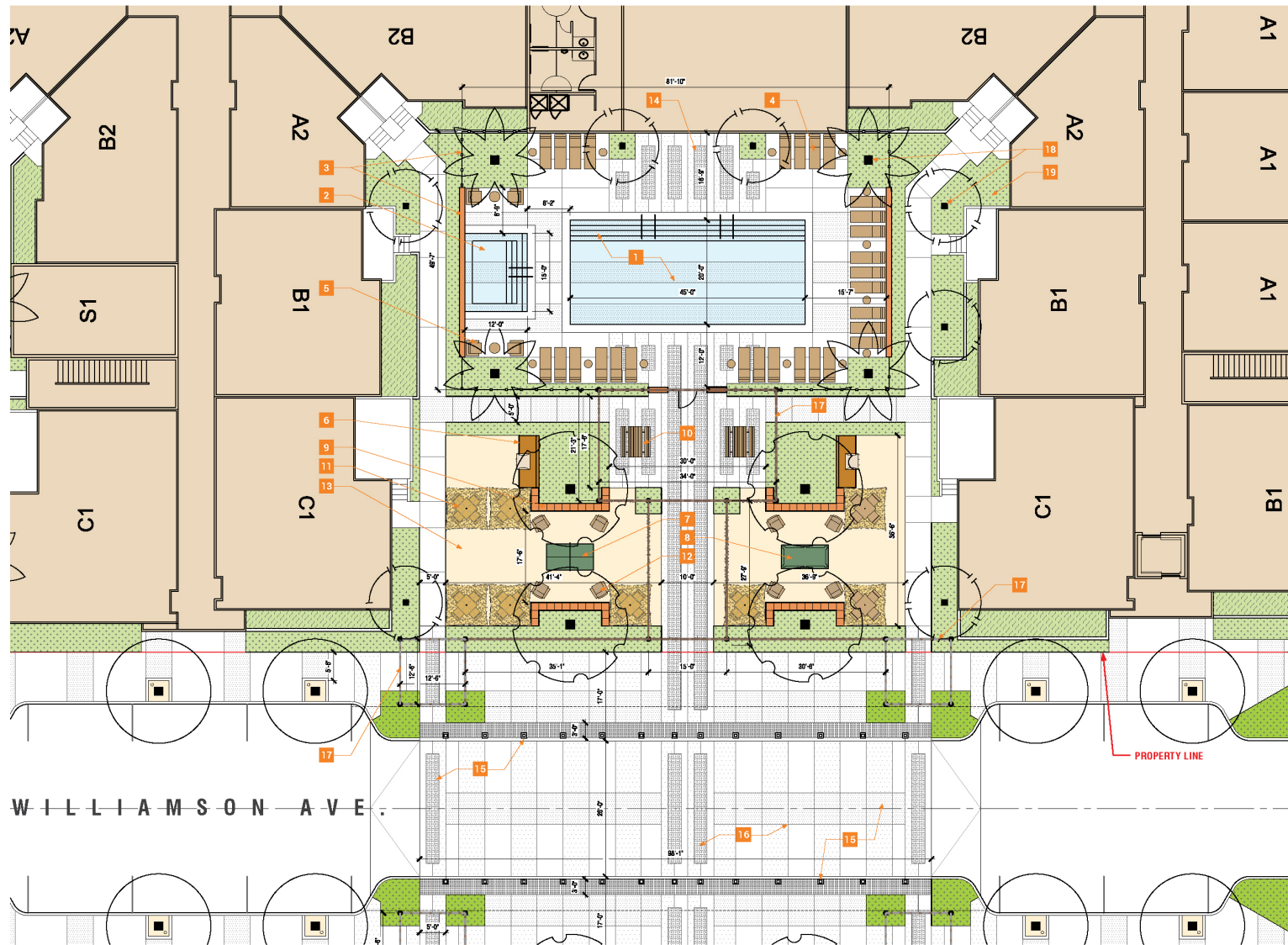


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Source: Land Studio 360

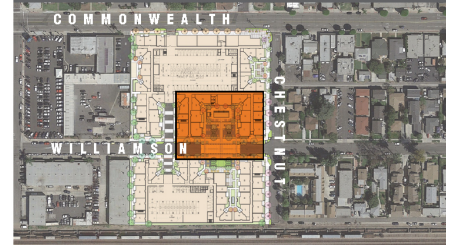
Figure 3-13: Plazas and Open Spaces, Building 1



LEGEND

- 1 900 sqft pool
- 2 180 sqft spa
- 3 6' h. non-climbable pool enclosure fencing and walls - see sheet L-7
- 4 Chaise lounges
- 5 Lounge seating and ADA accessible seating
- 6 Barbecue counters (2)
- 7 Ping pong table (weather resistant materials) (anchored in place)
- 8 Billiards table (weather resistant materials) (anchored in place)
- 9 Seatwalls
- 10 Picnic tables
- 11 Dining tables with umbrellas and ADA accessible seating
- 12 Adirondack-style chairs
- 13 Stabilized decomposed granite paving
- 14 Railroad-inspired thematic paving
- 15 Speed table flush with pedestrian paving + safety bollards
- 16 Linear paving motif on speed table
- 17 Overhead "rail" feature with lighting
- 18 Trees and Palms - see sheet L-8
- 19 Shrub and groundcover planting areas - plant list on sheet L-9

KEYMAP

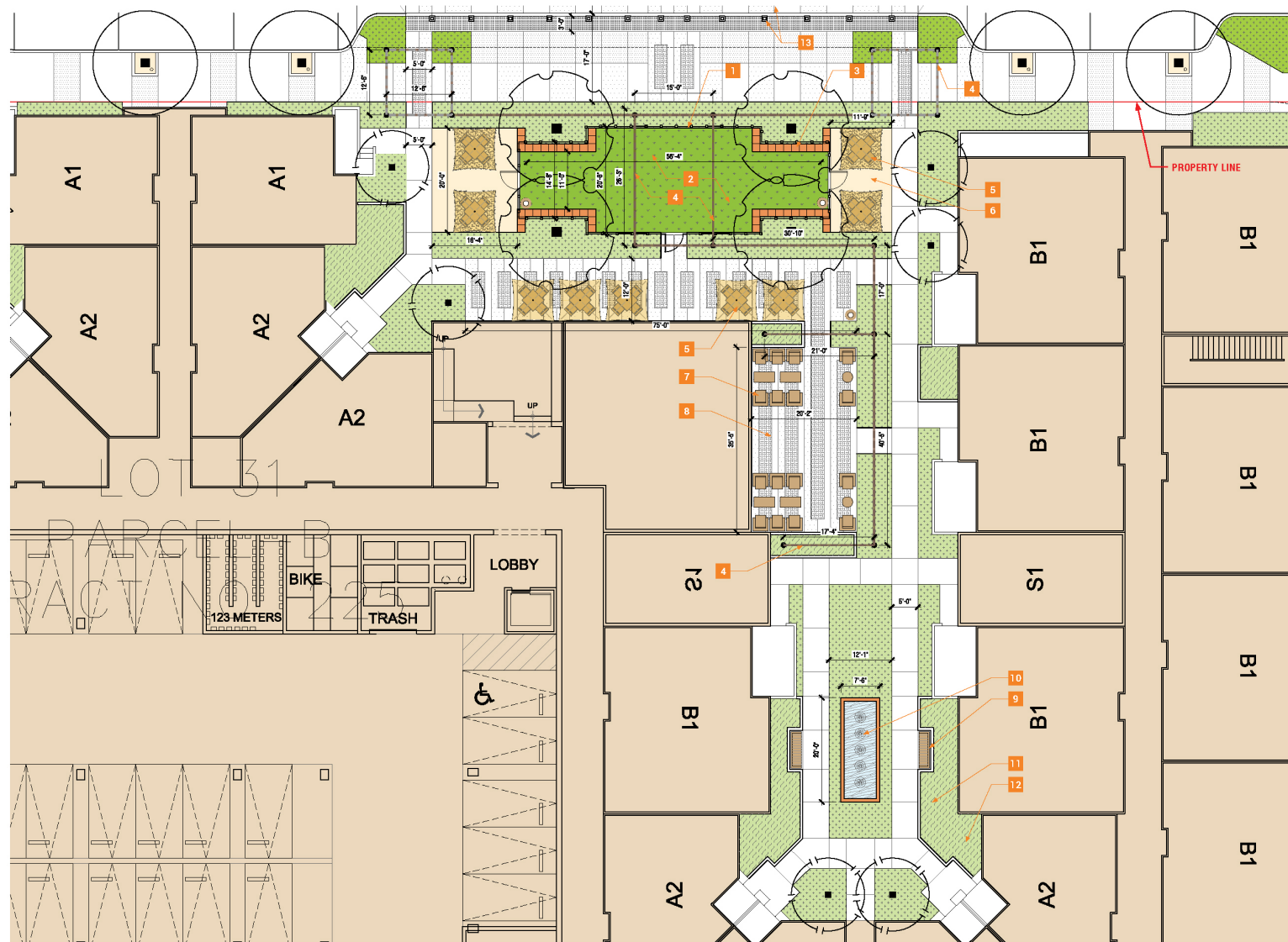


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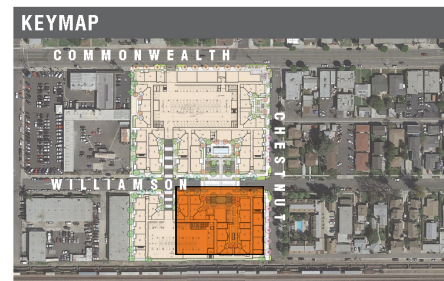
Source: Land Studio 360

Figure 3-14: Pool Courtyard, Building 1



LEGEND

- 1 1,200 sqft gated dog park w/ 3' h. fencing - see sheet L-7
- 2 Artificial turf
- 3 Seatwalls
- 4 Overhead "rail" feature with lighting
- 5 Dining tables with umbrellas and ADA accessible seating
- 6 Stabilized decomposed granite paving
- 7 Lounge seating and ADA accessible seating
- 8 Railroad-inspired thematic paving
- 9 Passive garden courtyard with benches and ADA accessible seating
- 10 Water feature
- 11 Trees - see sheet L-8
- 12 Shrub and groundcover planting areas - plant list on sheet L-9
- 13 Speed table flush with pedestrian paving + safety bollards

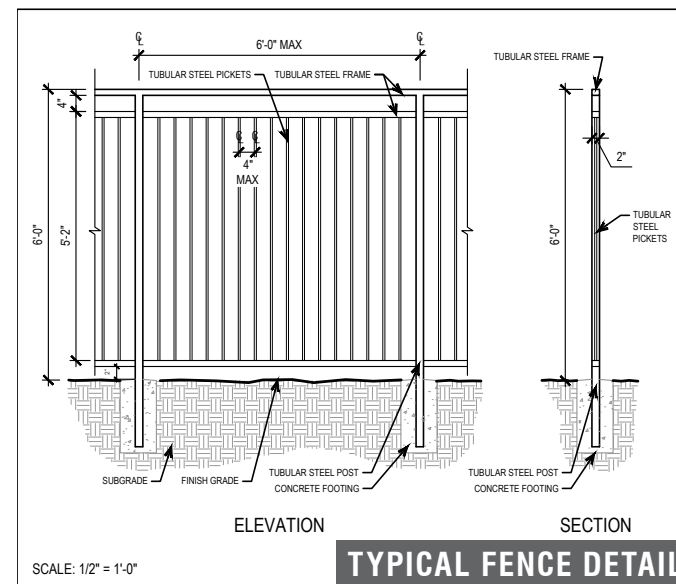
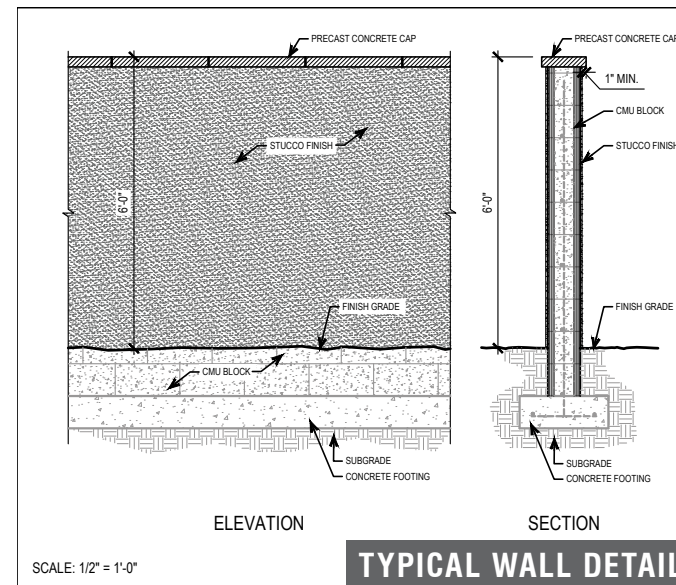
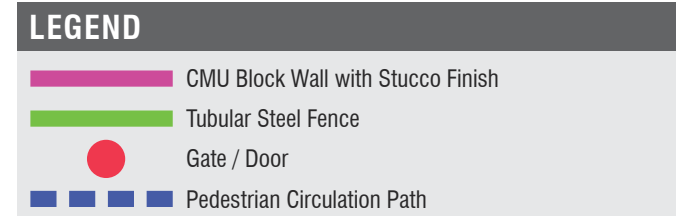
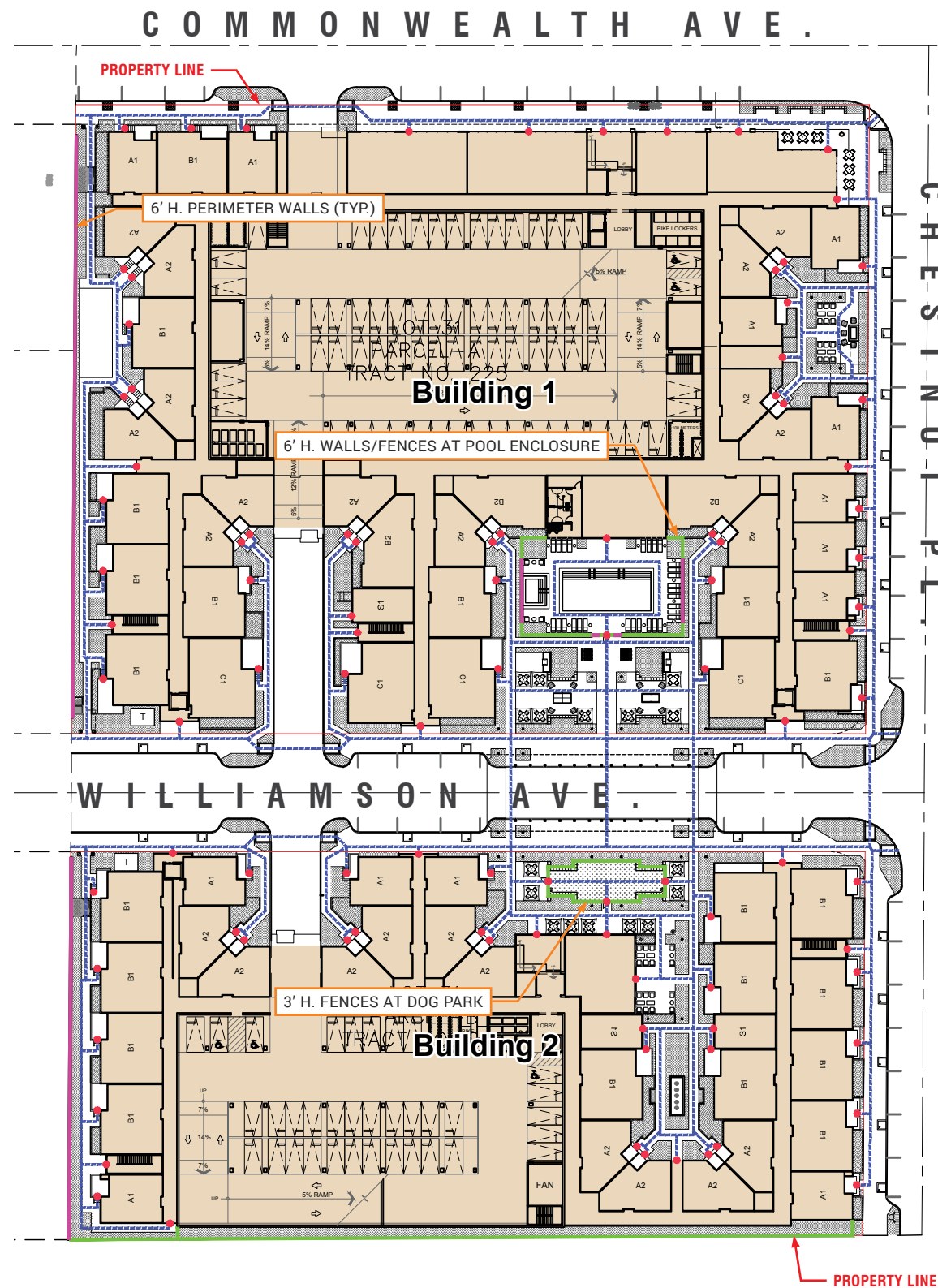


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Source: Land Studio 360

Figure 3-15: South Courtyard, Building 2



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Source: Land Studio 360

Figure 3-16: Walls and Fences

4 **INFRASTRUCTURE AND SERVICES**

This section explains the infrastructure requirements for the project, including water and sewer, drainage, and water quality. Public services plans are also described.

This section explains the infrastructure requirements for the project, including drainage, water and sewer, and grading. Public services plans are also described.

4.1 INFRASTRUCTURE PLAN

4.1.1 Water

Domestic water is currently provided to the project site by the City of Fullerton. Existing water lines ranging from 4 to 12 inches are located in West Commonwealth Avenue, Williamson Avenue, and Chestnut Place, as shown in Figure 4-1, *Water and Wastewater Plan*.

- Building 1 on Parcel A will be connected to an existing 12-inch water line in West Commonwealth Avenue by two domestic water laterals and one 8-inch fire water lateral.
- Building 2 on Parcel B will be connected to an existing 8-inch water line in Williamson Avenue by one domestic water lateral and one 8-inch fire water lateral.

It is also anticipated that the existing 4-inch water line in Chestnut Place, between West Commonwealth Avenue and Williamson Avenue, will be replaced with an 8-inch water line and will be extended south to the terminus of Chestnut Place. Other anticipated improvements include the relocation of the fire hydrant located at the southwest corner of the intersection of West Commonwealth Avenue and Chestnut Place, and installation of one proposed fire hydrant at the south terminus of Chestnut Place. In addition to the two proposed fire hydrants, one existing fire hydrant in West Commonwealth Place and two existing fire hydrants in Williamson Avenue will be utilized to serve the development. Proposed water improvements are shown in Figure 4-1, *Water and Wastewater Plan*.

4.1.2 Wastewater

Sanitary sewer service is currently provided to the project site by the City of Fullerton. Sewer treatment is provided by the Orange County Sanitation District treatment plant in Huntington Beach.

Existing sanitary sewer lines ranging from 6 to 12 inches are located in West Commonwealth Avenue, Williamson Avenue, and Chestnut Place, as shown in Figure 4-1, *Water and Wastewater Plan*. A Sewer Capacity Analysis report, dated February 16, 2016 and prepared by Fuscoe Engineering, Inc., indicates that the 12-inch VCP (vitrified clay pipe) located in West Commonwealth Avenue runs westerly at 0.21%, the 8-inch VCP located in Williamson Avenue runs westerly at 0.48%, and the 6-inch VCP located in Chestnut Place runs northerly at 0.14%. The project site currently discharges wastewater into the existing 8-inch sanitary sewer line located in Williamson Avenue through six 4-inch VCP laterals.

It is anticipated that both Buildings 1 and 2 will discharge wastewater into the existing 8-inch sanitary sewer line located in Williamson Avenue through two 8-inch laterals. In addition, Building 1 will also discharge wastewater into the existing 12-inch sanitary sewer line located in West Commonwealth Avenue through two 8-inch and one 4-inch lateral. It is anticipated that the proposed 4-inch lateral will connect to a proposed grease interceptor and serve the proposed retail space located at the northeast corner of Building 1.

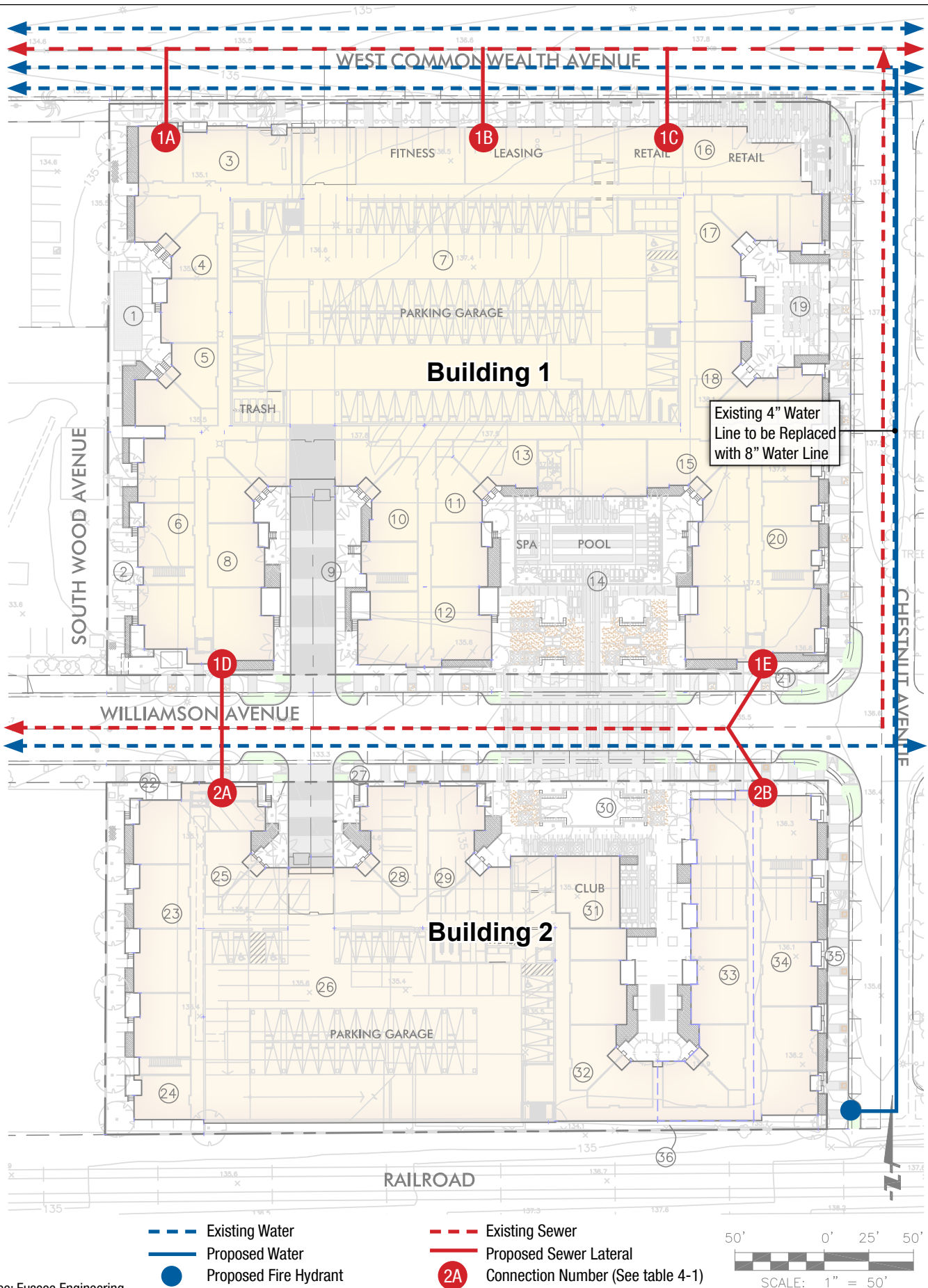


Figure 4-1: Water and Wastewater Plan

Other anticipated sanitary sewer improvements include installation of two proposed manholes in West Commonwealth Avenue and one proposed manhole in Williamson Avenue. Wastewater generation flows, per the Sewer Capacity Analysis report, are summarized below in Table 4-1, *Wastewater Generation Flows*. Proposed sanitary sewer improvements are shown in Figure 4-1, *Water and Wastewater Plan*.

Table 4-1 Wastewater Generation Flows				
Connection		Wastewater Flow (gpd)		
Lateral Size	Discharges to	Existing Peak	Proposed New	Proposed Total
8"	12" West Commonwealth Avenue	0	24,469	24,469
8"	12" West Commonwealth Avenue	0	24,469	24,469
4"	12" West Commonwealth Avenue	0	4,250	4,250
8"	8" Williamson Avenue	501	24,469	23,968
8"	8" Williamson Avenue	501	24,469	23,968
8"	8" Williamson Avenue	501	33,375	32,874
8"	8" Williamson Avenue	501	33,375	32,874

4.1.3 Drainage and Water Quality

Drainage

Storm drainage and flood control in Fullerton is maintained by the Orange County Flood Control District (OCFCD). The project site is a part of the San Gabriel River/Coyote Creek watershed and is not located within a FEMA 100-year flood zone. Existing and proposed drainage and water quality features are shown in Figure 4-2, *Drainage and Water Quality Plan*.

Storm drains are currently not present within the streets adjacent to the project site. Under existing conditions, runoff from both parcels A and B surface flows northerly towards West Commonwealth Avenue and Williamson Avenue respectively. The runoff is then conveyed westerly through concrete curb and gutters within the road right-of way for approximately 2,500 feet towards Euclid Avenue. At this location, the runoff is intercepted by catch basin inlets which connect into an existing public storm drain system. The public storm drain system discharges into the Fullerton Creek Channel (Facility No. A03) which is a reinforced concrete trapezoidal channel that is owned and maintained by OCFCD. Fullerton Creek Channel flows westerly and connects into Coyote Creek which is a reinforced concrete trapezoidal channel that is owned and maintained by Los Angeles County Flood Control District (LACFCD). Coyote Creek discharges into the San Gabriel and ultimately drains out to San Pedro Bay. The project site does not receive any off-site runoff.

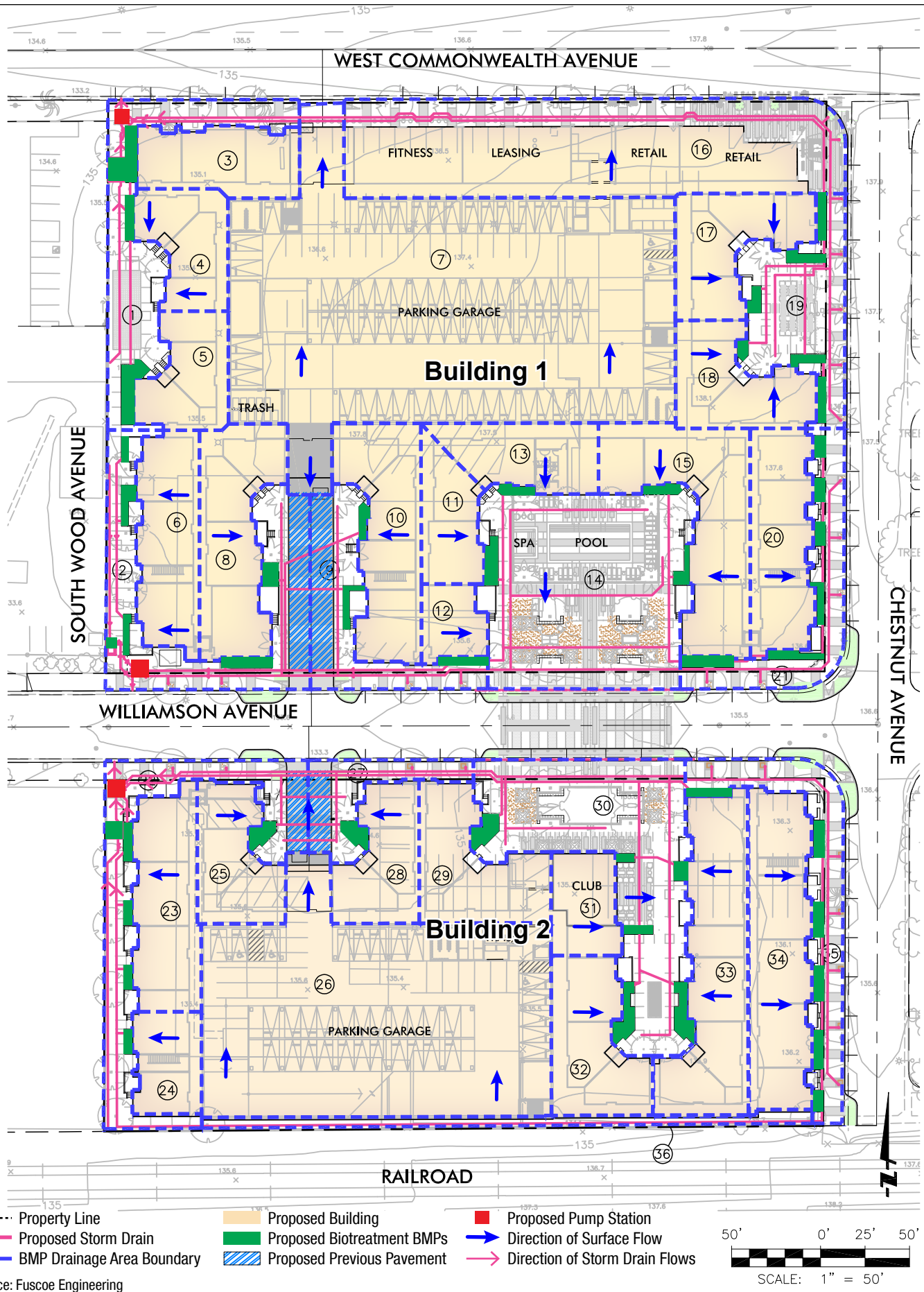


Figure 4-2: Drainage and Water Quality Plan

Project drainage will be conveyed in a similar manner to existing conditions except that site runoff will be directed through a proposed private on-site storm drain prior to discharging into West Commonwealth Avenue and Williamson Avenue. Low flows will be diverted through either a Modular Wetland System or bioretention planter for biotreatment and then pumped out onto either West Commonwealth Avenue or Williamson Avenue. High flows will bypass the biotreatment BMPs and be pumped directly onto the public street.

Water Quality

The project is required to comply with Chapter 12.18, Water Quality Ordinance, of the City's Municipal Code. This section requires the preparation of a Water Quality Management Plan (WQMP). A Preliminary Water Quality Management Plan has been prepared for the project by Fuscoe Engineering, Inc, dated May 2016. The Preliminary WQMP identifies potential pollutant sources associated with the project that may affect the quality of discharges of storm water from the site. The Preliminary WQMP also includes the following selection of Best Management Practices (BMPs) that will be utilized for the project:

Site Design BMPs

- Minimize Impervious Area
- Preserve Existing Drainage Patterns and Time of Concentration
- Disconnect Impervious Areas

Low Impact Development (LID)/Treatment Control BMPs

- Bioretention Planters with Underdrains
- Modular Wetland Systems

Hydrologic Source Controls (HSCs)

- Impervious Area Reduction (permeable pavers)

Prior to issuance of any precise grading permits, an updated WQMP will need to be submitted for review and approval to the Public Works Department of the City of Fullerton. The updated WQMP will describe the final selection of BMPs for the project site.

In compliance with the Clean Water Act (CWA) and the National Pollutant Discharge Elimination System (NPDES) program, the project site will be required to obtain coverage under the statewide General Permit for Discharges of Storm Water Associated with Construction Activity (Construction General Permit, 2009-0009-DWQ). The General Permit requires that a Storm Water Pollution Prevention Plan (SWPPP) be prepared for the anticipated construction activities. The SWPPP describes the erosion and runoff control measures to be used during and after construction, and provides a plan to inspect and maintain these control measures.

4.1.4 Utilities

Southern California Edison (SCE) provides electricity to the project site through existing power lines on the site along the western property line. The existing on-site vaults, lines, and easements would be abandoned and removed as part of the project. New lines would be provided to connect to existing off-site SCE facilities adjacent to the site.

The Southern California Gas Company (SoCalGas) provides natural gas to the project site.

Telephone and data services are provided by Verizon and cable television services are provided by Charter Communications. Service connections for the proposed project would be made from existing utility lines, with new utility lines placed underground.

4.2 PUBLIC SERVICES

4.2.1 Fire and Police

Fire Service

The Fullerton Fire Department provides fire protection and emergency services to the project site. The nearest fire station is Station No. 1 (Headquarters) located approximately 0.9 mile east of the project site at 312 East Commonwealth Avenue. According to The Fullerton Plan EIR, the Fire Department's goal is to reach the location of all medical aids and fires within five minutes. The Fire Department is currently meeting or exceeding that goal.

The proposed project would be required to comply with the Fullerton Fire Prevention Ordinance (Chapter 13 of the City's Municipal Code), which addresses fire prevention and includes the City's Fire Code as well as with the City's Building Code (Chapter 14 of the City's Municipal Code), which adopts the California Building Code and other codes related to building construction, in order to prevent the creation of fire hazards in the City.

Police Service

The Fullerton Police Department provides police protection services to the City. The Police Department operates from one station located at 237 West Commonwealth Street, located approximately 0.4 mile east of the project site. Although a new mixed-use development would be constructed and operated on the project site, the project would be located in an urbanized area and would not result in a substantial increase in demand on police service.

4.2.2 Solid Waste

Solid waste collection and disposal is provided by Republic Services under contract to the City of Fullerton. All proposed development with the Specific Plan will be required to consult with the City regarding the design and location of trash bin enclosures and frequency for collection services.

4.2.3 Schools

The project site is located within the boundaries of the Fullerton School District (FSD) and the Fullerton Joint Union High School District (FJUHSD).

- The FSD provides educational services for students in kindergarten through eighth grade. The FSD serves approximately 13,700 students in a 26-square-mile area, and has 15 elementary schools; two kindergarten through eighth grade schools; and 2 junior high schools.
- The FJUHSD provides educational services for students in 9th to 12th grade. The FJUHSD serves a 50-square-mile area that includes the elementary districts of Buena Park, Fullerton, La Habra, and

Lowell Joint. Six, four-year comprehensive high schools are operated by the FJUHS: Buena Park, Fullerton, La Habra, Sonora, Sunny Hills, and Troy. La Vista High School, a continuation high school and La Sierra High School, an alternative high school, also serve FJUHS students. As of the 2013/2014 school year, there were 13,772 students in the FJUHS.

The project site is within the attendance boundaries of Golden Hills Elementary School, Nicolas Junior High School, and Fullerton Union High School. With the development of up to 295 residential units, it is estimated that the project would generate up to approximately 148 elementary and middle school students and approximately 54 high school students.

New development within the Specific Plan area will be conditioned to pay statutory school impact fees prior to issuance of building permits.

5

IMPLEMENTATION

This section discusses the development review procedures of the City and other relevant permitting agencies applicable to the Specific Plan Area. Implementation of the proposed land uses, including Specific Plan adoption, subsequent approvals and plans, and phasing are outlined in this chapter. Additionally, financing sources and maintenance responsibilities are identified.

The purpose of this chapter is to provide an outline of the steps necessary to implement the Specific Plan and applicable regulations in coordination with the City of Fullerton and other governing public agencies. This chapter is intended to address each of these elements for the benefit of the development team, the City and other relevant agencies, and interested citizens.

5.1 ADMINISTRATION

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to cities to utilize Specific Plans for purposes of implementing the goals and policies of the City's General Plan.

This Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan area.

5.1.1 Responsibility

The City of Fullerton's Community Development Department, its Director or their designee shall be responsible for administering the 600 West Commonwealth Specific Plan in accordance with the provisions of this Specific Plan document, all governing and applicable state and federal laws, the City of Fullerton's General Plan, and the City's Municipal Code.

5.1.2 Applicability

All development within the 600 West Commonwealth Specific Plan area shall comply with the requirements and standards set forth in this Specific Plan document. If conflicts exist between the standards contained in this Specific Plan and those found in the City of Fullerton General Plan, Zoning Ordinance or Municipal Code, the regulations and standards in the Specific Plan shall take precedence. Any area of site development, administration, review procedures, landscaping requirements, and regulations not expressly addressed by this Specific Plan document shall be subject to the provisions of the City of Fullerton Zoning Code, Municipal Code or General Plan, using the context and objectives of the Specific Plan as a guide.

5.1.3 Enforcement

The 600 West Commonwealth Specific Plan serves as the implementation tool for the zoning for the Specific Plan area. The Specific Plan addresses permitted uses, development standards, and project design guidelines.

The City shall enforce the provisions of the Specific Plan in the same manner that the City enforces the provisions of the General Plan, Municipal Code and Zoning Code.

5.1.4 Severability

If any portion of this Specific Plan and its regulations are declared to be invalid or ineffective in whole or in part by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions thereof.

5.1.5 Interpretation

Whenever the provisions contained in the Specific Plan conflict with the Municipal or Zoning Codes, the provisions of the adopted Specific Plan shall take precedence. Any ambiguity concerning the content or application of the 600 West Commonwealth Specific Plan shall be resolved by the City's Community Development Director, or their designee. Such interpretations shall take into account the stated goals and intent of the Specific Plan.

5.1.6 Initial Entitlements

Initial entitlements required for development of the Specific Plan area include the following actions to be taken by the City:

- Environmental Impact Analysis – The 600 West Commonwealth Specific Plan is a discretionary project and is subject to the requirements of the California Environmental Quality Act (“CEQA”). As part of the approval process for the Specific Plan, an Environmental Impact analysis must be considered and certified by the City prior to approval of any of the project-related entitlements. This is anticipated to take the form of a Mitigated Negative Declaration.
- General Plan Amendment – Parcel A is designated “Commercial” and Parcel B is designated “Industrial.” The proposed General Plan Amendment would change the General Plan land use designation on the project site (Parcel A and Parcel B) to “Urban Center Mixed-Use”.
- Zoning Amendment – Parcel A is zoned General Commercial (G-C), and Parcel B is zoned Manufacturing, General (M-G). The proposed zoning amendment would change the zoning designation on the project site (Parcel A and Parcel B) to Specific Plan District (SPD).
- Specific Plan – The 600 West Commonwealth Specific Plan is a regulatory document that establishes the zoning, land use designations, development standards, and design guidelines for the entire Specific Plan area. The Specific Plan will implement the City's General Plan as amended. The Specific Plan will be considered by the Planning Commission and adopted by Ordinance by the City Council. The Specific Plan shall be adopted concurrently with or after the adoption of the SPD zoning designation on the project site and after a General Plan Amendment.
- Major Site Plan. Consistent with Section 15.47.040 of the City's Municipal Code, the proposed project requires review and approval of a Major Site Plan.

5.1.7 Substantial Conformance

Final development plans for each parcel of the project may be adjusted or modified based on final design and engineering and the precise development plans of the planning area builder. Substantial Conformance is a mechanism to allow the approval of minor modifications for development under the Specific Plan.

Written documentation requesting a proposed minor modification to support an implementing site plan, or use permit must be submitted for the review and approval of the Community Development Department, its Director or their designee.

Minor Modifications under Substantial Conformance

The Community Development Director or their designee shall have the authority to approve minor adjustments or modifications, as defined herein, which substantially conform to the approved Specific Plan through an administrative “Substantial Conformance” review process, so long as those minor modification and adjustments are consistent with the intent of the Specific Plan.

Minor modifications may be warranted to accommodate changes resulting from final design and engineering that cause adjustments in location of utilities or other infrastructure, development of innovative product design, or other similar modifications deemed to be minor. Minor modifications or technical adjustments may include, but are not limited to the following:

- Inclusion of land uses not specified in Section 3, Development Regulations but similar in intent and character subject to interpretation by the Community Development Director as specified in the Development Regulations in Section 2 of this document;
- Modifications necessary to comply with final Conditions of Approval or mitigation measures when adopted under concurrent or subsequent actions;
- Addition of information to the Specific Plan (including maps or text) for purposes of clarification that does not change the intent of any plan or regulation, as well as correction of any clerical or grammatical errors;
- Adjustments to the alignment, location and sizing of utilities and facilities or a change in utility and/or public service provider may be approved by the City’s Engineering or Public Works Department so long as the adjustments or changes are found to be in compliance with applicable plans and standards of the agency responsible for such utilities and facilities and do not result in new or different environmental impacts;
- Minor changes to the architectural or landscape design guidelines, which are intended to be conceptual in nature and flexible in implementation;
- Modification of any design element in this Specific Plan that improves circulation, reduces grading, improves drainage, improves infrastructure, or provides similar utility and reduces operations and maintenance costs or improves the level of sustainability;

The minor modifications described and listed above are not comprehensive. Any modification that is deemed by the Community Development Director to be in substantial conformance with the purpose and intent of the Specific Plan shall be permitted.

The documentation of substantial conformance may include text and/or maps which describe the nature of all proposed modifications or adjustments to the Specific Plan. This application of substantial conformance with the adopted Specific Plan shall undergo any necessary technical review by City agencies as the Community Development Director or their designee deems necessary.

Substantial Conformance Determination shall not include significant modifications in the basic design of the Specific Plan area including any increase in the allowable units within the Specific Plan area.

Action

No public hearing shall be required for a finding of Substantial Conformance. The Community Development Director or designee shall be the review and approval authority for a finding of Substantial Conformance. The Director's findings shall be provided by written notice to the Applicant approving, conditionally approving, or denying the determination of Substantial Conformance. The Director's decision shall be final, subject to the appeal procedures established by the Fullerton Municipal Code.

Findings

The following findings shall be required for a Substantial Conformance Determination:

- The modifications are consistent with the goals and intent of the Specific Plan;
- The physical characteristics of the site have been adequately assessed, and proposed building sites are of adequate size and shape to accommodate proposed uses and all other features of development;
- There is supporting infrastructure, existing or available, consistent with the requirements of the Specific Plan, to accommodate the development without lowering service levels; and
- The development resulting from the Substantial Conformance Determination will not have a substantial adverse effect on surrounding property or the permitted use thereof, and will be compatible with existing and planned land uses, as well as the character of the surrounding area.

5.1.8 Amendments to the Specific Plan

Substantial modifications to the Specific Plan would require an Amendment. A minor modification or adjustment to the Specific Plan listed in the section above would not require a Specific Plan Amendment. An amendment to the Specific Plan is required if the following occur:

- Any increase in the overall development density thresholds within the Specific Plan; or
- Any addition of new land uses not contemplated by the Specific Plan's Development Regulations.

5.1.9 Appeals

An appeal of any determination, decision, or requirement of City staff or Planning Commission shall be made in conformance to the appeal procedures established by the Fullerton Municipal Code.

5.2 IMPLEMENTATION

5.2.1 Specific Plan Adoption

The 600 West Commonwealth Specific Plan will be prepared, submitted, and approved in a manner consistent with California Government Section 65451, as well as Chapter 15.21 of the City's Zoning Code. The Specific Plan shall be adopted by ordinance and shall serve as the zoning for the Specific Plan area. The approved Specific Plan project site will be designated on the City's Zoning Map as the Specific Plan District (SPD). The land use and development standards identified in this Specific Plan document

supersede all zoning regulations to the extent that they would be in conflict with the sections of this Specific Plan.

5.2.2 Subsequent Approvals and Plans

Several levels of subsequent approvals are required to implement the project after adoption of the Specific Plan.

- Site Plan review based on a builder-level development plan (may be approved concurrently with the Specific Plan);
- A Signage Program will be prepared to provide for design continuity within the new community.

In order to develop any uses listed as “C” (Conditional Use Permit) in Section 2.4.2: Conditional Uses, the Applicant shall submit an application for a Conditional Use Permit in accordance with the requirements set forth in the Municipal Code, as may be modified or supplemented by this Specific Plan. Findings for approval of a Conditional Use Permit shall be as outlined in the Municipal Code.

5.3 PHASING

Construction of the proposed project may be progressively implemented in stages, provided that vehicular access, public facilities, and infrastructure are constructed to adequately service the development, or as needed for public health and safety. Construction activities would be initiated with demolition of on-site structures and parking areas. All infrastructure within the Specific Plan boundary will be installed in one phase at project initiation. These improvements include rough grading, storm drain, water, sewer, dry utilities, and street improvements.

5.4 MAINTENANCE AND OWNERSHIP

Maintenance of facilities within the Specific Plan area will be accomplished through a combination of public and private mechanisms. Generally, facilities dedicated to public agencies will be maintained by that agency, while private facilities will be maintained by a business association. Table 5-1, *Financing, Ownership, and Maintenance* outlines the anticipated program.

A Business Association or other association may be formed to address shared driveways, landscaping, common open space, signage, and maintenance within the commercial and multifamily areas of the Specific Plan.

Table 5-1
Financing, Ownership, and Maintenance

Improvement	Financing	Ownership	Maintenance
Water Improvements -In public rights-of-way -On private property	Developer Developer	Public Private	City Private/City ¹
Sewer Improvements -In public rights-of-way -On private property	Developer Developer	OCSD Private	OCSD Private/OCSD ²
Drainage System	Developer	Private	Private
Water Quality Features	Developer	Private	Private
Public Street Improvements	Developer	City	City
Private Internal Drives and Parking	Developer	Private	Private
Landscaping within Public Right-of-Way	Developer	City	City
Common Area	Developer	Private	Private
Private Recreation Area	Developer	Private	Private

5.5 FINANCING

Development within this Specific Plan is private in nature and will be financed by the project applicant and/or developer with private capital investment. No public/City funds will be used for development. The project will be privately owned, and its owners will be responsible for ongoing maintenance and operational costs.

5.6 RELATIONSHIP TO CEQA

An Environmental document is required prior to adoption of this Specific Plan to analyze potentially significant environmental impacts of the project in compliance with the provision of CEQA. This document, determined to be a Mitigated Negative Declaration (MND), will analyze the Specific Plan development program and address potential impacts associated with the development. The MND will include recommended mitigation measures and analyze implementing actions for the development. The adopted MND will fulfill the requirements for environmental documentation for most subsequent discretionary and ministerial applications for development within the Specific Plan area.

¹ City maintenance will extend to the water meter, which is on private property.

² OCSD maintenance of the sewer system will extend to the sewer cleanout, which is on private property.

As Lead Agency, the City of Fullerton will implement a monitoring program for approved mitigation measures. To assist in this monitoring effort, a Mitigation Monitoring Program will be developed and approved by the City as part of environmental findings and attached to the Final Specific Plan upon approval.

6

APPENDICES

- Legal Description
- Mitigation Monitoring and Reporting Program (when Available)

APPENDIX A LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF FULLERTON, IN THE COUNTY OF ORANGE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A:

THE WESTERLY 100.00 FEET OF LOT 31 OF TRACT NO. 225, AS PER MAP RECORDED IN BOOK 13, PAGE 28, MISCELLANEOUS MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THEREFROM THAT PORTION LYING SOUTHERLY OF THE NORTHERLY LINE OF WILLIAMSON AVENUE, 60.00 FEET IN WIDTH, AS DESCRIBED IN A DEED TO THE CITY OF FULLERTON, A MUNICIPAL CORPORATION, RECORDED FEBRUARY 26, 1964 IN BOOK 6939, PAGE 53, OFFICIAL RECORDS.

PARCEL B:

LOT 31 OF TRACT NO. 225, IN THE CITY OF FULLERTON, COUNTY OF ORANGE, STATE OF CALIFORNIA, ACCORDING TO THE MAP FILED IN BOOK 13, PAGE 28, OF MISCELLANEOUS MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPT THE PORTION OF THE WEST 100.00 FEET THAT LIES NORTH OF THE SOUTH LINE OF WILLIAMSON AVENUE, 60.00 FEET WIDE, AS DESCRIBED IN THE DEED TO THE CITY OF FULLERTON, A MUNICIPAL CORPORATION, RECORDED FEBRUARY 26, 1964 IN BOOK 6939, PAGE 53, OF OFFICIAL RECORDS.

APN: 032-081-26, 032-081-29, 032-081-30, 032-081-10 AND 032-084-04